

IV. RESULTS

HISTORIC ARCHITECTURE

Bridge 447A was identified by an imprint of its designated number located on the northwest corner of the west wing wall. The bridge carries Fleming's Landing Road (State Route 9, formerly Road 449) across a shallow tributary of Cedar Swamp (Plate 1). The bridge, constructed in 1934 or 1935, is a single span reinforced concrete slab structure supported on concrete abutments with nearly parallel wing walls. The bridge is 23 feet in length, with a span of 8 feet, 4 inches, and an overall height of about 9 feet. The stream that the bridge spans is confined in a gravel base culvert with sloping sides, which obscure the bridge's concrete abutments. The bridge is 30 feet wide at the northern end and 32 feet at the southern end to accommodate a curve in the road, setting the two sides of the bridge slightly out of parallel alignment.

The bridge span is marked by a balustrade with three rectangular openings between plain squared concrete balusters beneath a flat top rail. The wing walls flanking the balustrade are 7 feet, 4 inches each in length with chamfered top corners and one inset diamond each on the exterior walls. Currently painted white, Bridge 447A was designed to handle a 20-ton truckload

It was during the transition of authority from the New Castle County Office of the Engineer to the State Highway Department that Bridge 447A was approved and constructed in 1934. The New Castle County Office of the County Engineer drew up plans on July 6, 1934 and jurisdiction was transferred to the State Highway Department by a Levy Court resolution dated July 17, 1934. It is perhaps due to this shift in authority and associated transfer of documents that the records on Bridge 447A were misfiled or lost.

The only reference to this bridge is a plan and elevation drawing on file at the DelDOT Engineering Office. This plan of 447A was produced under the authority of the Levy Court, which oversaw the construction of roads, bridges, sewers, sidewalks, curbs, and gutters in New Castle County. The county engineering office designed the bridge, but it may actually have been built by the State Highway Department Bridge Division. The bridge division, under the leadership of State Bridge Engineer Arthur Livingston from 1918 through 1948, had been responsible for the construction of bridges and culverts on Delaware's highways and major roadways, inheriting responsibility for smaller roads after 1934. No record of construction for Bridge 447A was identified in the highway department's annual Tabulations of Contracts Awarded for the years 1932 through 1945 (Annual Reports of the State Highway Department: 1932 - 1945).

During 1934, 39 bridges and 453 culverts were constructed in Delaware. These were constructed with as much non-mechanized labor as possible, as were the roads built during this year, to provide as much work as possible to the unemployed. Federal employees under one of the works programs apparently did not construct Bridge 447A. Civil Works Administration (CWA) workers had only been used in the winter months of 1933 - 1934; the bridge was built after July 1934.

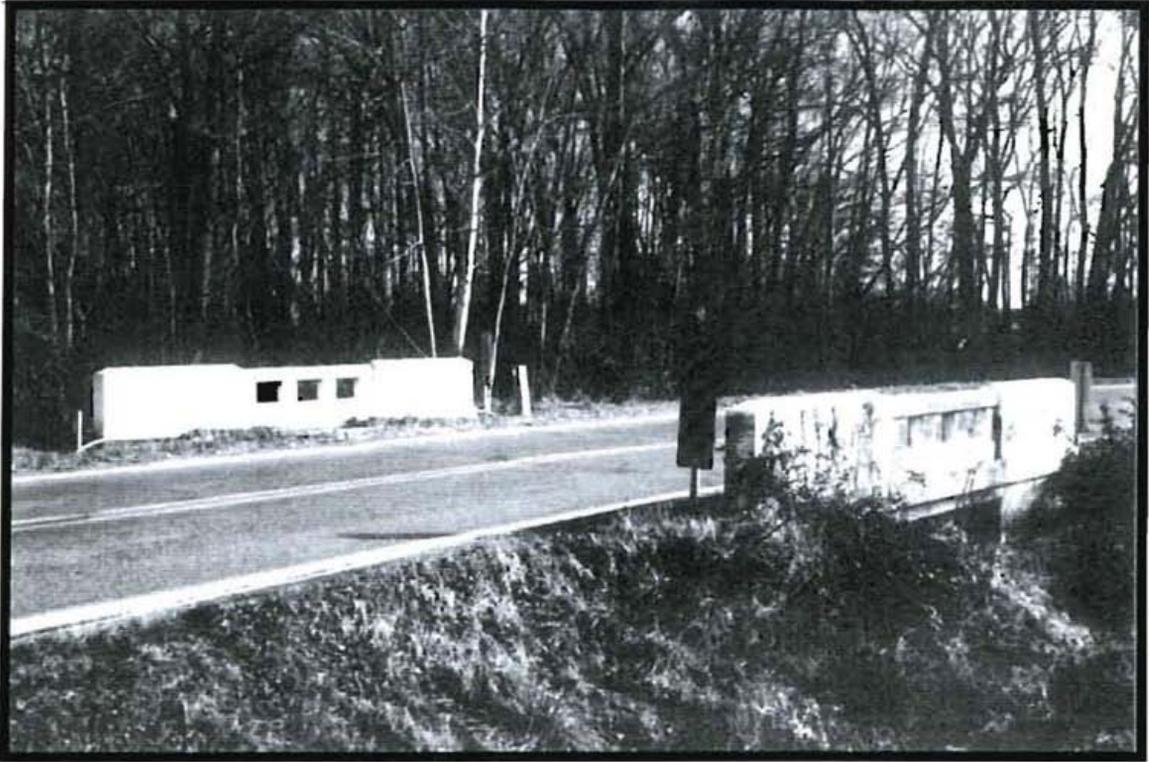


Plate 1 Bridge 447A, View Looking Southeast

Bridge 447A was likely constructed to improve transportation for farmers in the area. Truck farming had developed early in Delaware as a profitable form of agribusiness, but was dependent on favorable modes of transportation. Peaches were grown in the area surrounding Bridge 447A and may have been transported over State Route 9, formerly Road 449. The improvements to this road and the construction to the bridge may also have been effective in supporting small tourist business, which provided amenities to beach visitors of the Delaware River and Bay.

The brick culvert is a utilitarian structure likely constructed by the homeowner whose driveway it conveys over a drainage ditch. The construction date is difficult to determine due to the culvert's utilitarian nature and lack of identifiable style or construction technique; its condition and materials, however, indicate that the culvert was probably constructed between 1930 and 1945. The culvert, located just south of the intersection of Fleming's Landing Road and Taylor's Bridge Road, carries a driveway over a drainage ditch—as mentioned above—that parallels Fleming's Landing Road (see Figure 2).

The bottom of the culvert was inaccessible, but appears to be of poured concrete. The culvert's arch, which supports the gravel driveway, is constructed of brick roughly pointed with a cement mortar (Plate 2). The inferiority of the construction indicates that a trained mason probably did not build the culvert. Again, the property owner more likely built the culvert to ease transit from Fleming's Landing Road over a drainage ditch to the main house and outbuildings.

ARCHAEOLOGY

A total of 32 shovel tests were excavated within the project's APE, and located within three intersections proposed for improvements. Two areas containing aboveground cultural resources were also investigated. Each section will be discussed individually.

State Route 9 and Paddock Road Intersection

Section A – Southern Area. Section A is a small, flat triangular patch of land situated at the southernmost extent of the State Route 9 and Paddock Road intersection (Figure 6). The area is bounded on all sides by roadway. A total of 6 shovel test pits (STPs) at five-meter intervals were excavated in this location. Shovel tests A1 to A4 were set approximately 3 meters from the edge of the road. STPs A5 and A6 were placed 5 meters off of A2 and A3, respectively. STPs A2, A5, and A6 yielded one or two pieces each of modern amber and green bottle glass from fill levels; these artifacts were discarded in the field. A single brick fragment was recovered from STP A3 (also from fill) and subsequently discarded.

A pull-tab, probably roadside debris, was retained from the top level of STP A4. As expected, layers of fill capping subsoil characterize the profiles in this area. Typically the subsoil in this area is a 2.5Y 3/2, a very dark grayish brown, silty clay.

Section B – Northern Area. This area, just north of Section A, comprises the southernmost tip of a large cornfield. A total of three STPs at 5-meter intervals were excavated in Area B (see Figure 6). Disturbance from a tractor tire prevented the excavation of any STPs beyond B3. No



Plate 2 Brick Culvert, View Looking North

cultural material was recovered from any of the shovel tests in this area, with the exception of a discarded piece of modern green bottle glass from the top of stratum of B1. Soil profiles from this area were generally characterized by plowzone of 10YR 5/3 brown silt loam capping subsoil of 2.5Y 5/3 light olive brown, silty clay mottled with 10YR 5/2 grayish brown silty clay.

State Route 9 and Thoroughfare Neck Road Intersection

Section C – Southwest Area. This section, located at the southwest corner of the intersection of State Route 9 and Thoroughfare Neck Road, is bounded on the east and west by roadway, a deep drainage ditch, and by a cornfield to the south. Two shovel tests at an interval of 5 meters were excavated in Area C (Figure 7). STP C1 is approximately 5 meters from the intersection point of State Route 9 and Thoroughfare Neck Road. A single artifact from C2, a post-1904 machine-made bottle base, was recovered from this section.

Section D – Southeast Area. A total of 7 shovel tests at five and ten-meter intervals were excavated in this section, located at the southeast corner of the intersection of State Route 9 and Thoroughfare Neck Road (see Figure 7). Initially, three STPs (D1 to D3) at 5-meter intervals were excavated to test the corner of the intersection. No cultural material was recovered from STP D1 or D2. Artifacts recovered from D3 (including brick, a piece of window glass, and a piece of post-1904 clear bottle glass) prompted the excavation of four additional shovel tests. D4 was laid in at 10 meters southeast of D2, and contained brick, mortar, wire nail, modern amber, and clear bottle glass. D5 was five meters southeast from D4 and contained brick, window glass, a ceramic tile sherd, several molded rim and body pieces from a glass vase, a key, and machine-made bottle glass, post-1904. The area becomes brushy and overgrown approximately 5 meters beyond D5. In shovel test D6, 5 meters south of D3, archaeologists uncovered brick, mortar, window glass, modern bottle glass, a flowerpot sherd, and one porcelain sherd. D7 was located 10 meters from D6 and marked an obvious decline in artifacts, with one brick fragment and one worn piece of glass. No intact features were located in this area, although approximate boundaries of artifact concentration can be inferred from the positive STPs as extending roughly 20 meters north-south and 10 meters east-west.

Section E – Northwest Area. Six shovel tests were excavated in Section E, located in a cornfield on the northwest corner of the intersection of State Route 9 and Thoroughfare Neck Road (see Figure 7). All shovel tests in this area were placed at 5-meter intervals, with the exception of E6, placed 10 meters northwest of E3. Soil profiles in this area consisted of a plowzone with an average depth of 15 centimeters capping a mottled silty clay subsoil. No cultural material was recovered from this area.

Section F – Northeast Area. This section is located on the northeast corner of the State Route 9 and Thoroughfare Neck Road intersection. Two shovel tests spaced 5 meters apart were excavated to test this area (see Figure 7). Shovel test F1 was 5 meters east from the edge of State Route 9, while STP F2 was 5 meters north from the edge of Thoroughfare Neck Road. Two pieces of machine-made bottle glass with a stippled surface were collected from STP F2.

State Route 9 and Sawmill Branch Road Intersection

Section G – Southwest Area. Section G is located on the southwest corner of the State Route 9 and Sawmill Branch Road intersection. The property is occupied by a trailer home, which sits approximately 50 meters from the intersection. The testable area is comprised primarily of loose pebbles and gravel with sparse grassy spots that appeared to be used as a driveway. Due to the disturbed nature of the area, no shovel tests were excavated in this location.

State Route 9 Stations 4·100 to 4·150

Section H – West Area. Section H is adjacent to State Route 9 immediately south of a small tributary of Cedar Swamp, and is located on property owned by the Vietnam Veterans Motorcycle Club of Delaware (Figure 8). This area marks the northernmost tested portion of the project area. Three shovel tests were excavated at 5-meter intervals. The area appeared to have been recently disturbed, as evidenced by tire and possible backhoe tracks, sparse plantings of grass, and pebbles on the ground surface; this was later confirmed by one of the club members. The three STPs exhibited layers of fill capping that appeared to be sterile intact soil. Artifacts recovered from the fill layers of STP H1 and H2 include brick, window glass, and coal slag.

Section I – East Area. Section I is located directly across State Route 9 from Section H. Two head and footstones belonging to Jeremiah Cales (d.1879) and Judith Cales (d. 1868), previously identified, are located six meters off the edge of the road and approximately 50 meters south of the Bridge 447A (Plate 3) (Figure 9). The area, which is very heavily overgrown, was cleared of under brush; no other grave features were identified in close proximity to those already found. The inscription on the first tombstone reads, “Our Mother – Judith Cales – Died November 19th, 1868 – Aged 69 Years” (Plate 4). The second is inscribed, “Our Father-Jeremiah Cales – Died October 18th, 1879 – Aged 76 Years” (Plate 5).

Preliminary research into the identification of the two grave sites indicated that they are located halfway between the villages of Taylor’s Bridge and Deakynville, near the site of the “African Church” indicated on both the Beers and Baist maps (see Figures 3 and 4). The African Church was actually the Scott Methodist Episcopal Chapel. The first record of the church dates back to June 3, 1864, when a group of trustees paid \$10 for a half-acre lot “in Thoroughfare Neck” on the “public road from Taylors Bridge to Bartons Landing” (State Highway 9) (New Castle County Deed Book Y7:499). The lot was part of the farm of Isaac Staats, member of a large landowning family, and appears to be the same lot as Tax Parcel 15-008.00-019, designated as a cemetery in current tax records (see Figure 9). Preliminary research indicates that the lot was occupied by the church building; no extant burials are known to be in the lot. The tombstones of Judith and Jeremiah “Cales” (see discussion of inscription below) are located on an adjoining parcel of land.

Bishop Levi Scott, for whom the church was named, dedicated Scott’s Chapel on July 21, 1867. Scott was one of the leaders of the Methodist Church in America and had established the first Methodist Conference in Africa. Itinerant ministers from the Smyrna Circuit serviced the chapel. The original chapel was replaced by a new, one-story frame building dedicated in 1898, marking the beginning of the tenure of the church’s first settled clergyman, the Reverend



Plate 3 Section I, Head and Footstones of Jeremiah and Judith "Cales"

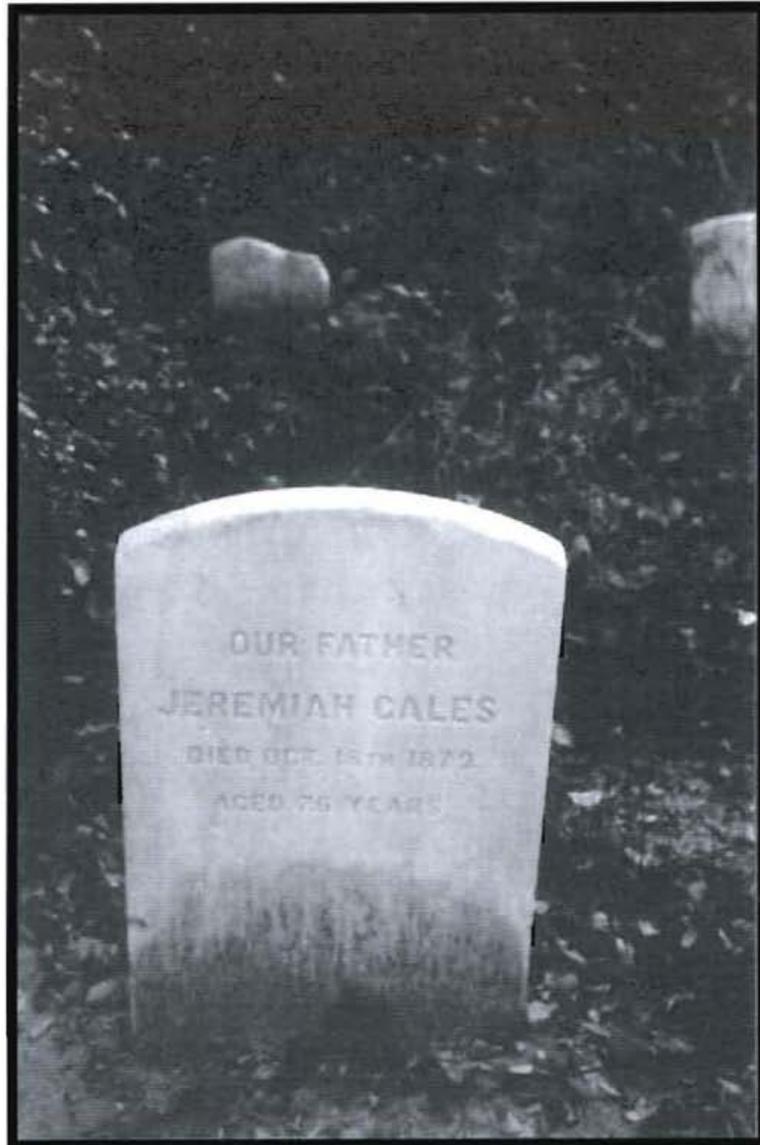


Plate 4 Section I, Headstone of Jeremiah "Cales,"
View Looking East

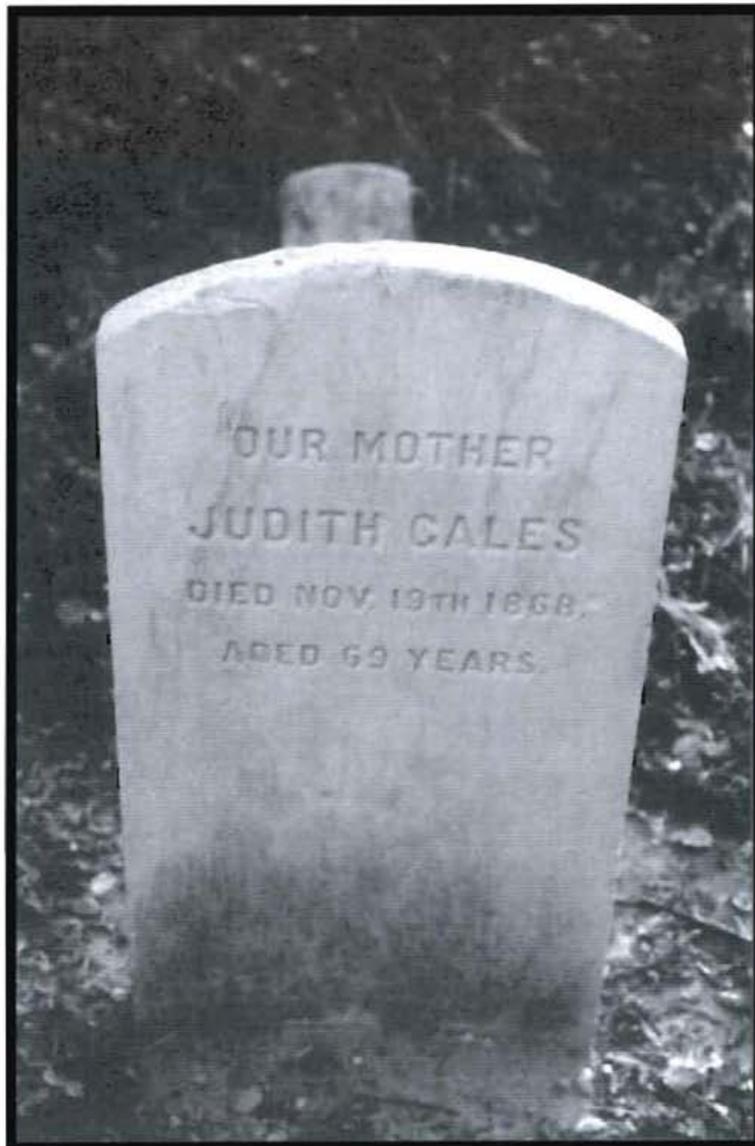


Plate 5 Section I, Headstone of Judith "Cales,"
View Looking East

George Jones, and also the church's incorporation as the Scott ME Church of Blackbird (New Castle County Deed Book U17:500; WPA undated; Zebley 1947:190).

Among the trustees named in the original church deed were Jeremiah Gales, Arthur Bungy, Isaac Ayres, Absalom Reading, Simon Farmer, Joseph Gales, and Jacob Bungy (New Castle County Deed Book Y7:499). Jerry Gale and his wife, Judy, headed up a household in Appoquinimink Hundred in 1860. The census taker recorded Jerry Gale as a 56-year-old farmer in 1860, who owned no real estate, but did possess \$500 in personal property. The Gales' household included I. Gale, a 20-year-old farmhand, S. Wilson, a 28-year-old farmhand, M.A. Wilson, a 17-year-old domestic, a 15-year-old girl named E. Archy, and a 6-year-old boy named J. Auxon. All members of the Gale household were African American and had been born in Delaware (US Bureau of the Census, Appoquinimink Hundred, 1860:781).

Judith Gale died in 1868. Jeremiah never remarried, and shared the house with Isaac and George Gale, undoubtedly two sons, a daughter-in-law, and a grandchild. The 1870 census enumeration for Jeremiah Gale suggests that he lived close to Colen Ferguson. Ferguson was a prosperous peach grower near the village of Blackbird who served as a state senator. When the Scott ME Chapel was incorporated in 1898, Ferguson was one of its trustees (New Castle County Deed Book U17:500; Scharf 1888:1027; US Bureau of the Census, Appoquinimink Hundred 1870:445).

Jeremiah Gale died in 1879 at the age of 76. A search of census records indicates that Jeremiah Gale had been a resident of Appoquinimink Hundred for at least four decades, having moved to the area from Smyrna. Various members of the Gale (alternately spelled Gales) family appear in the census records back to 1800, living in New Castle or Kent County (US Bureau of the Census, Appoquinimink Hundred 1850:215; 1840:316; Smyrna 1830:224; Pecader Hundred 1820:102, 110; Little Creek Hundred 1800:32).

The surname "Cales" appears to have been incorrectly carved on the tombstones in the project area. Further, a substantial amount of time apparently elapsed between interment and erection of these tombstones. If the tombstone had been erected at the time of Judith Gale's death in 1868, it would more likely be inscribed as "My Wife," rather than as "Our Mother." Jeremiah Gale lived another 11 years after his wife's death. Further research would be required to trace the children of Jeremiah and Judith Gale and their descendants. The story of the Gale family following the death of Jeremiah in 1879 might cast light on the tombstones themselves and the use of this location as a burial ground.

Approximately 10 meters north of the headstones, a deposit of brick and several large stones were visible on the surface. A number of modern mason jars and other glass containers were scattered throughout the area, which was cleared of brush and investigated through the use of five judgmentally placed shovel tests in order to determine approximate limits of this disturbance. The shovel tests revealed varying densities of brick and mortar. These shovel tests were not screened, although a sample of brick was retained as well as an ironstone sherd, possibly part of a wash basin, and a wood screw. This area measures roughly 12 meters north-south by 8 meters east-west, and is situated 8 meters from the edge of State Route 9 (see Figure

9). No intact foundation walls were identified. This section, including all head and footstones, was further documented with black-and-white photographs.

A broader walkover survey was conducted, as no additional gravesites were located in the immediate area of the graves of Jeremiah and Judith “Cales.” This survey resulted in the identification of one grave with an intact head and footstone and two rectangular shaped depressions, all oriented at similar east-west angles (see Figure 9). The headstone, roughly 45 meters northeast of the “Cales” graves, is engraved with “HARRIET wife of John Durham died July 29 1861 in the 32 year of her age” (Plate 6). No detailed information can be included for this headstone, as it was discovered after the initial research phase. However, it should be noted that the year of Harriet Durham’s death predates construction of the Scott ME Chapel, and nineteenth-century maps of the area do not include any Durhams among the local landowners. Two depressions, nearly identical in size and shape, were located approximately 4 meters west of Harriet Durham’s headstone (see Figure 9). The east-west orientation of these depressions and their proximity to the headstone indicate that these features are likely subsided grave shafts.

State Route 9 and Deaknyeville Road Intersection

Section J – Southeast. The southeast corner of this intersection is the location of a historic stone property marker (Figure 10). The marker is engraved on two sides: the west facing side reads “A. REED APRIL 5, 1861” (Plate 7), and the north side is engraved, “F. COOPER” (Plate 8). The property marker was recorded with black-and-white photographs and rubbings.

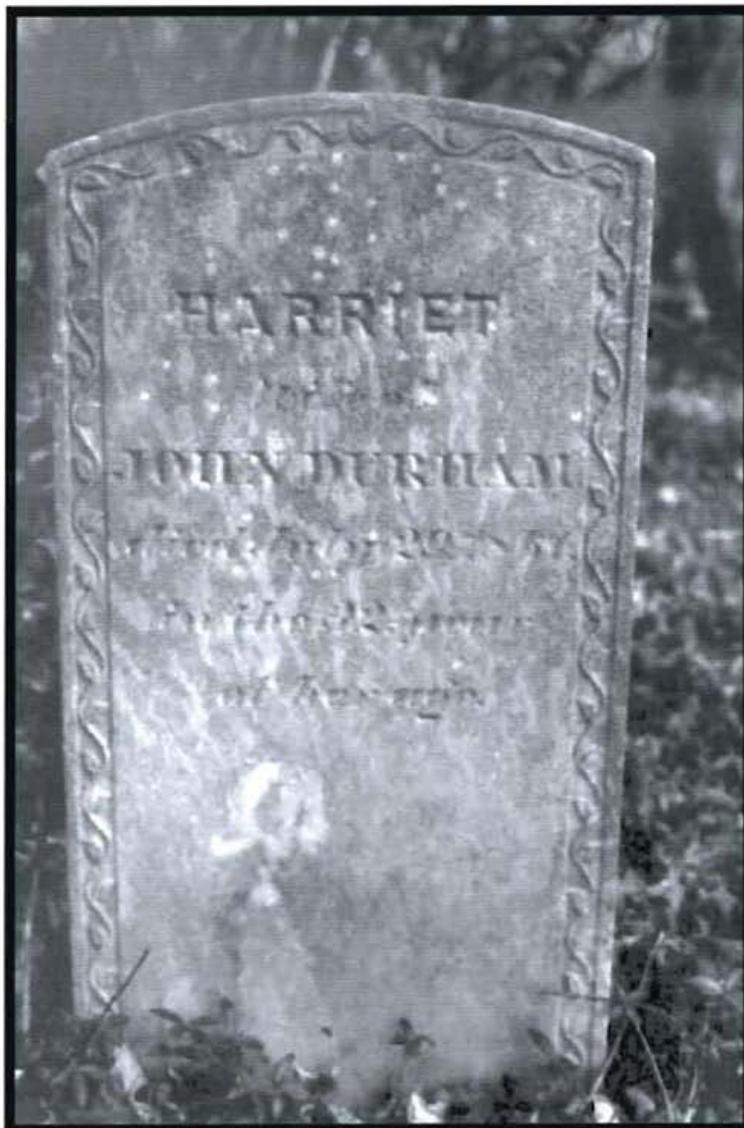


Plate 6

Section I, Headstone of Harriet Durham,
View Looking East (the Stone Reads:
"Harriet wife of John Durham died July 29
1861 in the 32 years of age")



Plate 7 Section J, Property Marker, View Looking East
(the Stone Reads: "A. Reed April 5 1861")

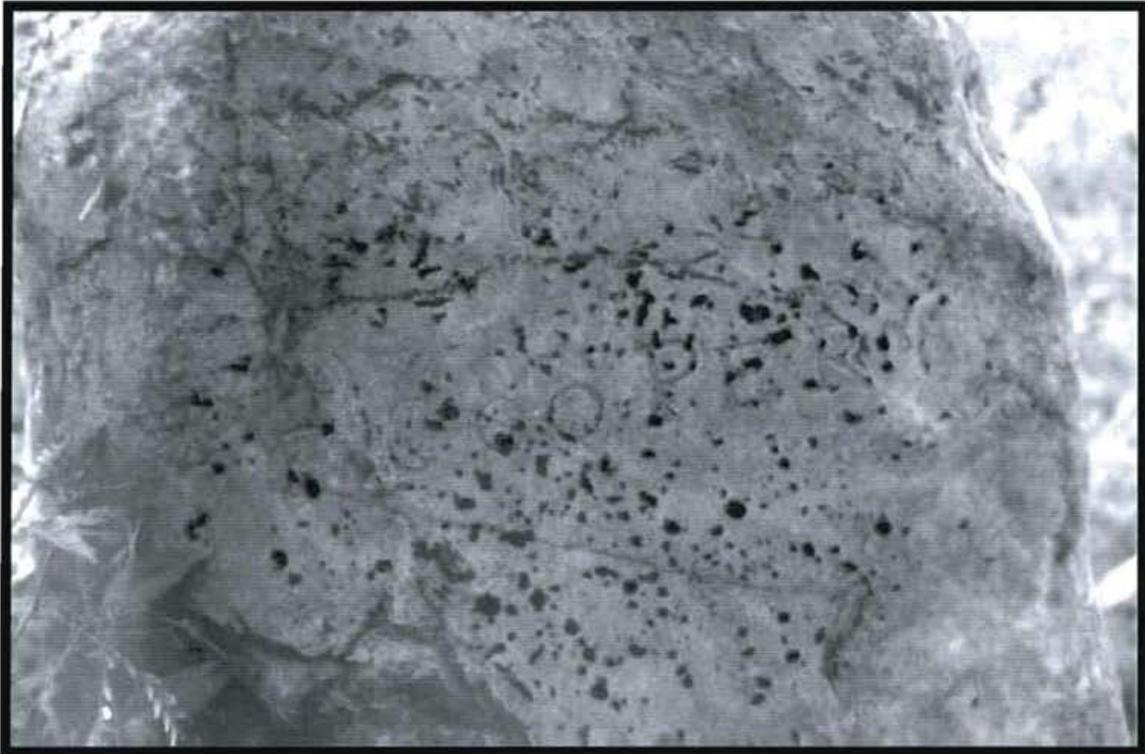


Plate 8 Section J, Property Marker, View Looking South
(the Stone Reads: "F. Cooper")