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Draft Report for:

## *Historic Architectural Survey*

*SR 1 from Five Points to Midway  
Sussex County, Delaware*



Prepared for:

Delaware Department of Transportation

On behalf of:

Whitman, Requardt and Associates, LLP

Submitted by:

*Kise Straw & Kolodner*

Architects

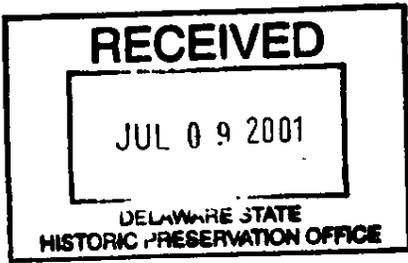
Planners

Historians

Archaeologists

*Judith F. Kennedy*

10 May 2001



S-8056 has 2  
addresses:  
43114 = none listed  
43904 = 1617 Coastal  
Survey Form = 1566 Coastal  
(photos match) 8/2010

Addresses don't match w/  
survey forms from 1987  
- S-8057  
- S-8056

8/2010

## ABSTRACT

This report documents the results and conclusions of a Phase I cultural resources survey along SR 1 from Five Points to SR 24 in Sussex County, DE. The Delaware Department of Transportation (DelDOT) proposes to make improvements to the west side of SR 1 from approximately the intersection of SR 1/Route 9 and SR 23, to SR 24 in Sussex County. As part of federal environmental requirements associated with these improvements, the firm of Whitman, Requardt and Associates, LLP (WRA) hired the Cultural Resources Group of Kise Straw & Kolodner, Inc. (KSK) to complete this survey on behalf of DelDOT. KSK conducted a survey of historic architectural (above-ground) resources and archaeological (below-ground) resources. Fieldwork was completed in the winter of 2000.

This study determined that there are seventeen (17) architectural resources fifty years and older within the Area of Potential Effects. All of the buildings were previously surveyed by John Milner & Associates (JMA) in 1988, as documented in DelDOT's Architectural and Cultural Series No. 79, *Cultural Resources Survey Delaware Route 1: Five Points to Rehoboth*. JMA determined that three of these resources met National Register criteria and those fourteen resources either did not meet National Register criteria or lacked integrity and could not convey significance. KSK, following field survey of the APE, determined that the three properties previously determined potentially eligible for the National Register no longer retained integrity. These three properties, therefore, in KSK's opinion could no longer be considered eligible for the National Register. KSK concurred with JMA's assessment and evaluation of the other fourteen properties and found no additional information to change these determinations. All seventeen properties appear to lack integrity, significance, or a combination of both qualities, and are therefore ineligible for the National Register of Historic Places. KSK identified no additional architectural resources within the APE.

This study revealed no recorded archaeological sites within the APE. A pedestrian survey of the area conducted by KSK revealed that approximately seventy percent of the project area was heavily impacted by recent commercial and retail development. A Phase I archaeological survey that encompassed the project area and included substantial shovel testing was conducted by JMA in 1988. This study was documented in DelDOT's Architectural and Cultural Series No. 79 as noted above. JMA did not identify any prehistoric or historic sites within the current APE. The road improvements proposed as part of this study will occur entirely within the area previously tested by JMA. Consequently, unless the area of potential ground disturbance changes, it is KSK's opinion that no additional archaeological work is required.

The APE contains no National Register eligible properties; therefore, the proposed improvements do not have the potential to affect cultural resources unless changes are made to the scope or location of the proposed improvements.

## TABLE OF CONTENTS

List of Figures

List of Plates

I.	Introduction.....	1
II.	Research Design .....	7
	<i>Research Objectives</i> .....	7
	<i>Methodology</i> .....	7
III.	Background Research .....	13
	<i>Geographic Setting</i> .....	13
	<i>Cultural History</i> .....	13
IV.	Description of Work .....	24
	<i>Archaeological Resources</i> .....	24
	<i>Architectural Resource Description and Evaluation</i> .....	24
V.	Summary.....	51
VI.	Bibliography .....	52
VII.	Vitae.....	56

## FIGURES

- Figure 1.** Project Area. (Source, the USGS *Lewes, Del. and Farimount, Del. Quadrangle*, 1984).
- Figure 2.** Architectural Area of Potential Effects. (Source, the USGS *Lewes, Del. and Farimount, Del. Quadrangle*, 1984).
- Figure 3.** Sussex County c. 1838. (Source, G. W. Boynton's *The First Two Railroads*, Bradford & Goodrich, 1838).
- Figure 4.** Lewes and Rehoboth Hundred c. 1850. (Source, Jacob Price and Samuel Rea's *Map of the State of Delaware*, Robert E. Smith, 1850).
- Figure 5.** Lewes and Rehoboth Beach Hundred, 1868. (Source, D.G. Beers' *Atlas of the State of Delaware*, Pomeroy & Beers, 1868).
- Figure 6.** SR1 from Five Points to Midway c. 1926. On file at the University of Delaware Spatial Lab (<http://bluehen.ags.udel.edu/spatlab>).
- Figure 7.** SR1 from Five Points to Midway c. 1938. On file at the University of Delaware Spatial Lab (<http://bluehen.ags.udel.edu/spatlab>).
- Figure 8.** SR1 from Five Points to Midway c. 1960. On file at the University of Delaware Spatial Lab (<http://bluehen.ags.udel.edu/spatlab>).

## PLATES

- Plate 1.** View east along SR 1 from Five Points.
- Plate 2.** View southeast along SR 1 from Shady Road.
- Plate 3.** View west along SR 1 from Marsh Road.
- Plate 4.** View southeast along SR 1 from Kings Highway.
- Plate 5.** View northwest along SR 1 from Dartmouth Drive.
- Plate 6.** View northwest along SR 1 from Kings Highway.
- Plate 7.** View southeast along SR 1 from Postal Lane.
- Plate 8.** View northwest along SR 1 from Ratcliffe Drive.
- Plate 9.** View southeast along SR 1 from Brian Drive.
- Plate 10.** View northwest along SR 1 from Wolf Neck Road.
- Plate 11.** View south to the abandoned Knapp Farmstead.
- Plate 12.** View northwest to the abandoned Knapp Farmstead
- Plate 13.** View west to demolished outbuildings associated with the Knapp Farmstead.
- Plate 14.** View south to 1310 Coastal Road.
- Plate 15.** View west to 1310 Coastal Road.
- Plate 16.** View south to 1408 Coastal Road.
- Plate 17.** View west to 1408 Coastal Road.
- Plate 18.** View south to 1410 Coastal Road showing transformation to a restaurant.
- Plate 19.** View southwest to 1410 Coastal showing new front addition.
- Plate 20.** View southwest to the Davidson House (lawyer's office) formerly a residential dwelling.
- Plate 21.** View southwest to the outbuilding associated with the Davidson House.
- Plate 22.** View south to 1414 Coastal Road (antique shop) formerly a single-family dwelling.
- Plate 23.** View west to 1414 Coastal Road.

- Plate 24.** View south to 1520 Coastal Road.
- Plate 25.** View west to 1520 Coastal Road.
- Plate 26.** View south to 1532 Coastal Road.
- Plate 27.** View west to 1532 Coastal Road.
- Plate 28.** View south to 1617 Coastal Road, now a retail shop and salon.
- Plate 29.** View west to 1617 Coastal Road.
- Plate 30.** View south to 2021 Coastal Road (funeral home).
- Plate 31.** View west to 2021 Coastal Road (funeral home)
- Plate 32.** View west to the Seafood Restaurant showing wraparound addition.
- Plate 33.** View south to the Seafood Restaurant, formerly a single-family dwelling.
- Plate 34.** View north to 1565 Coastal Road.
- Plate 35.** View east to 1565 Coastal Road.
- Plate 36.** View north to 1548 Coastal Road.
- Plate 37.** View east to 1548 Coastal Road with related outbuildings.
- Plate 38.** View north to 1683 Coastal Road.
- Plate 39.** View east to 1683 Coastal Road.
- Plate 40.** View north to the Givens House.
- Plate 41.** View east to the Givens House with related outbuildings.
- Plate 42.** View north to the Beaman House (antique shop) formerly a single-family dwelling.
- Plate 43.** View east to the Beaman House.
- Plate 44.** View west to the abandoned Carpenter Farmstead.
- Plate 45.** View northwest to the abandoned Carpenter Farmstead.



## I. INTRODUCTION

This report documents the results and conclusions of a Phase I cultural resources survey of SR 1 From Five Points to SR 24 in DE in Sussex County, Delaware. The Delaware Department of Transportation (DelDOT) proposes to make improvements along the west side of the exiting roadway to include an additional southbound travel lane. As part of federal environmental requirements associated with these improvements, the firm of Whitman, Requardt and Associates, LLP (WRA) hired the Cultural Resources Group of Kise Straw & Kolodner, Inc. (KSK) to complete this survey on behalf of DelDOT. KSK conducted a survey of historic architectural (above-ground) resources and archaeological (below-ground) resources. Fieldwork was completed in the winter of 2000.

This study has been conducted in accordance with and pursuant to federal laws and regulations. Federal law, specifically Sections 106 and 110 of the National Historic Preservation Act of 1966 (as amended), requires that federal agencies take into account how their undertakings may affect cultural resources. Additional applicable statutes and directives include Section 101 (b)(4) of the National Environmental Policy Act of 1966, as amended; 36 CFR Parts 60 and 63, "National Register of Historic Places;" 36 CFR Part 800, "Procedures for the Protection of Historic Properties;" the Archaeological Resources Protection Act (ARPA); and Title 29 (8705) and Title 7 (5302) of the Delaware State Code.

There are two components of this survey: a Phase I Archaeological Survey and a Historic Architectural Survey. The objectives of the Phase I Archaeological Survey are as follows:

1. Define the Area of Potential Effects (APE), or the geographic area within which the proposed improvements may cause changes in the character or use of archaeological sites (see Figure 1).
2. Identify and document all previously recorded archaeological resources located within the APE that have been listed in or determined eligible for listing in the National Register of Historic Places.
3. Confirm the presence or absence of intact, potentially eligible, archaeological resources located within the APE that have not been previously identified.

The objectives for the Survey of Historical Architectural Resources are as follows:

1. Define the Area of Potential Effects (APE), or the geographic area within which the proposed improvements may cause changes in the character or use of historic properties.
2. Identify and document all previously recorded historical architectural resources located within the APE that have been listed in or determined eligible for listing in the National Register of Historic Places.
3. Determine the potential effects, if any, that the proposed improvements may have on the National Register-eligible resources.

KSK completed the fieldwork in late fall and winter of 2000. All findings and analyses are documented and assembled in this report according to requirements present in *The Delaware*

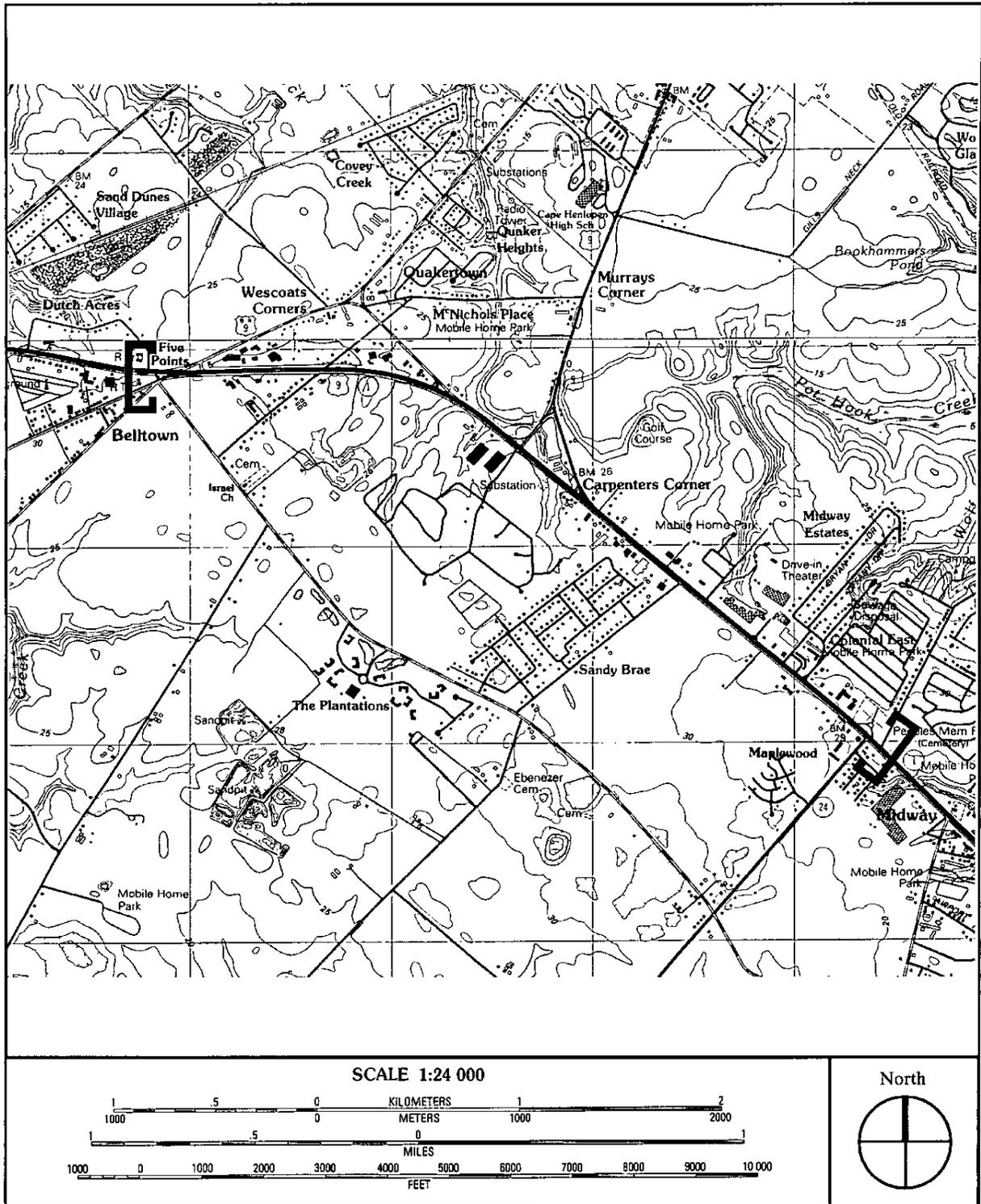
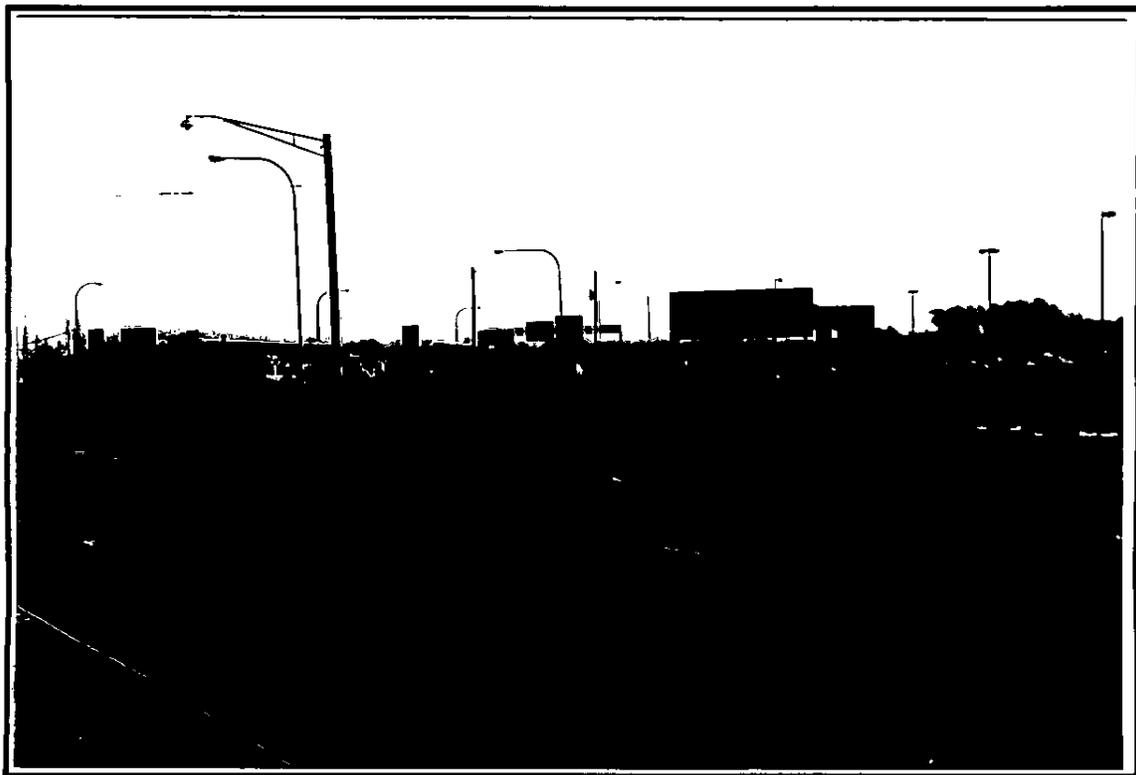


Figure 1. Project Area. (Source, the USGS Lewes, Del. and Farimount, Del. Quadrangle, 1984).

*Comprehensive Historic Preservation Plan.* This preliminary report is being submitted for review to WRA, DelDOT, and the Delaware State Historic Preservation Office (DE SHPO) with the understanding that upon the receipt of comments, the KSK will prepare the final report.

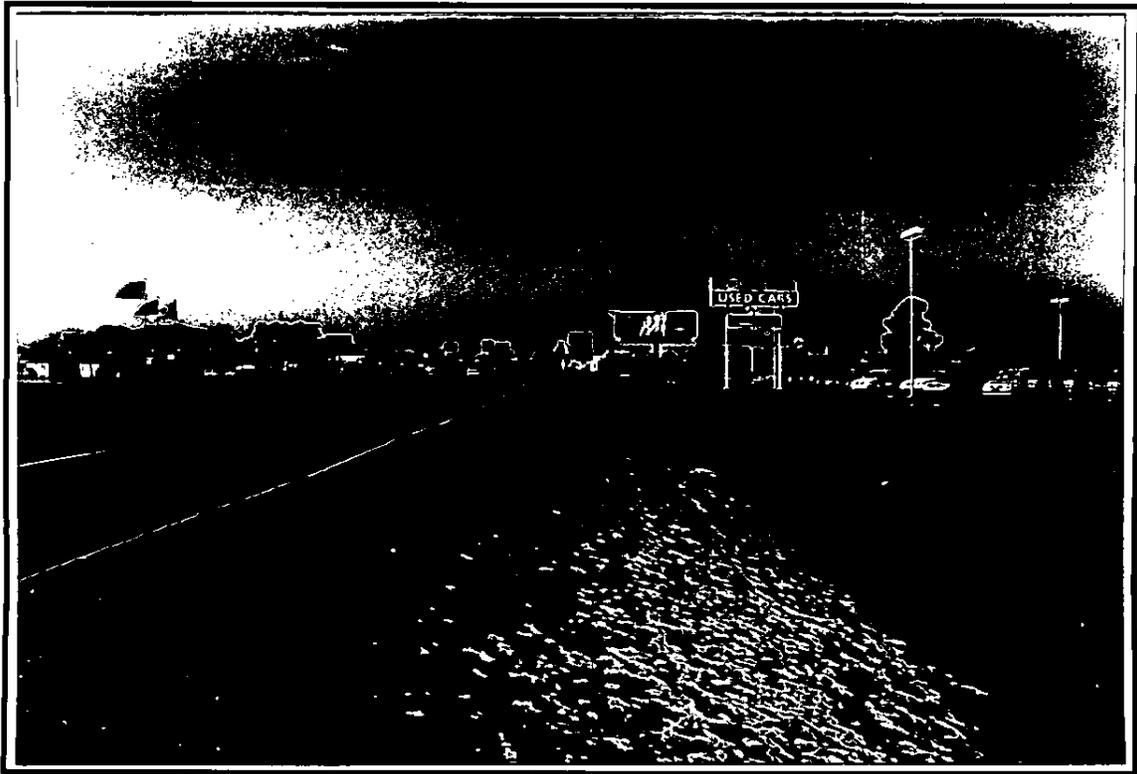
The cultural resources staff of Kise Straw & Kolodner extends its appreciation to all residents of the project area and to the staff of the various historical archives consulted for their assistance during the survey.



**Plate 1.** View east along SR 1 from Five Points.



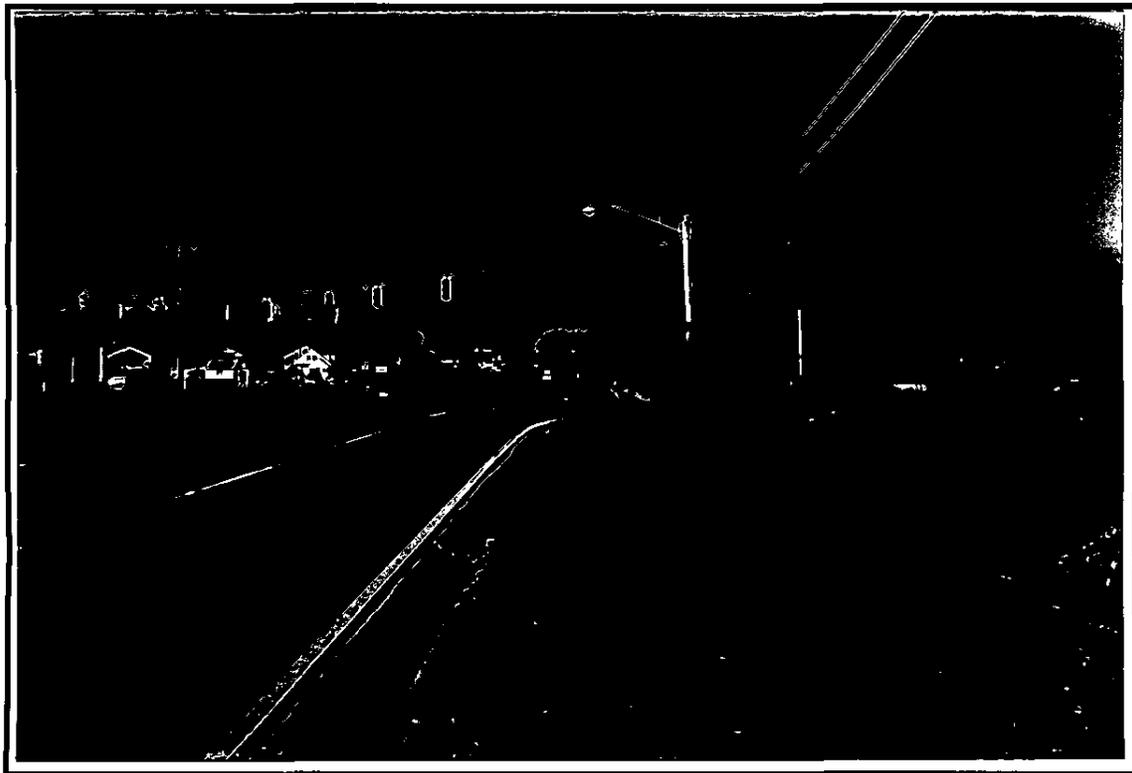
**Plate 2.** View southeast along SR 1 from Shady Road.



**Plate 3.** View west along SR 1 from Marsh Road.



**Plate 4.** View southeast along SR 1 from Kings Highway.



**Plate 5.** View northwest along SR 1 from Dartmouth Drive.



**Plate 6.** View northwest along SR 1 from Kings Highway.



## II. RESEARCH DESIGN

### Research Objectives

The research objectives for this project include:

1. Identification of previously recorded cultural resources in the project area and vicinity.
2. Preparation of a cultural history to provide background information about the project area and vicinity.
3. Documentation of cultural resources identified during fieldwork.
4. Correlation of cultural resources in the project area with the research questions, goals and priorities listed in *The Delaware Comprehensive Preservation Plan*.

### Methodology

#### *General Background Research*

KSK's project team conducted background research to gain an understanding of the historical and architectural development in the area and to identify and assess previously prepared documentation to avoid unnecessary duplication of efforts. KSK thoroughly searched pertinent literature to identify historical information about the project area and vicinity, and appropriate primary and secondary source materials pertaining to the area were examined. KSK consulted the Delaware State Historic Preservation Office (DESHPO); the Delaware State Archives; the Morris Library at the University of Delaware in Newark; the Historical Society of Pennsylvania in Philadelphia; and Van Pelt Library at the University of Pennsylvania in Philadelphia. KSK examined relevant historic maps and atlases, aerial photographs and local histories, and also reviewed previous cultural resources surveys and National Register nominations for properties, structures, and sites located within and near the project area. KSK also reviewed DelDOT's 1990 Architectural and Cultural Series No. 79, *Cultural Resources Survey Delaware Route 1: Five Points to Rehoboth*. This report was undertaken to assist in completing Section 106 compliance and NEPA requirements resulting from proposed highway improvements. It included both an architectural survey and Phase I archaeological testing along SR I/Route 9.

KSK then undertook field investigations to document the current appearance, condition, and integrity of potential resources within the APE. Each above-ground building fifty years and older was photographed, and field notes were taken describing the building's appearance, integrity, and proximity to the proposed improvements. KSK also conducted an intensive, systematic pedestrian survey of the entire project area to identify and photograph subsurface disturbances and areas possessing the potential to contain intact archaeological deposits. Field surveys were completed on January 24, 2001.

#### *Field Investigations of Archaeological Resources*

The goals of the Archaeological Survey were to document the archaeological resources previously recorded in the project area and to assess the likelihood that potentially significant

archaeological resources may be impacted by the proposed construction-related activities. This included establishing an appropriate Area of Potential Effects (APE). The APE is defined in 36 CFR 800.2(c) as "the geographic area or areas within which an undertaking may cause changes in the *character* or *use* of historic properties, if any such properties exist." The APE for archaeological resources was defined as those areas where the proposed improvements will result in subsurface disturbance.

The soils within the project area are classified as Fallsington and Sassafras Series soils (USDA 1970). The Fallsington Series soils are poorly drained soils normally found within upland areas. The Sassafras Series soils are deep, well-drained soils also found in upland areas.

There are no recorded archaeological sites within the project area; however, there are multiple sites located within a ½-mile radius of the APE. The Delaware State Cultural Resources Management Plan characterizes the area as having moderate to high potential to contain intact prehistoric deposits.

KSK conducted a pedestrian survey across the entire project area to document previous ground disturbance. Approximately seventy percent of the project area is heavily impacted by recent commercial and retail development.

#### *Field Investigation of Historic Architectural Resources*

In addition to the background research discussed above, the specific methodology for completing the survey of historic architectural resources included a windshield survey, field investigations, detailed research, and a National Register evaluation.

This project began with a windshield survey of the project area to determine if any historic architectural resources were present in the vicinity of the proposed improvements. Historic Architectural Resources are defined as properties containing buildings, districts, structures and objects constructed prior to 1950. The windshield survey revealed the presence of several historic architectural resources (Plates 1-10).

KSK staff examined project plans and established the APE for architectural resources. Based on the proposed improvements, the APE area was defined as including properties fronting the improvements or having sightlines to the improvements (see Figure 2).

KSK staff conducted field investigations within the APE to identify and document architectural resources constructed prior to 1950 that have not been previously recorded. Each resource identified during the fieldwork was photographed, and field notes were taken describing the resource's appearance, integrity, and proximity to the proposed improvements.

Using the data acquired during the research and field investigation phases, KSK staff evaluated each resource within the contexts outlines in the *Delaware Comprehensive Historic Preservation Plan* and the *Delaware Historic Context Master Reference and Summary*, and federal guidelines for determining National Register eligibility, 36 CFR 60.4 entitled "Criteria for Evaluation."



**Plate 7.** View southeast along SR 1 from Postal Lane.



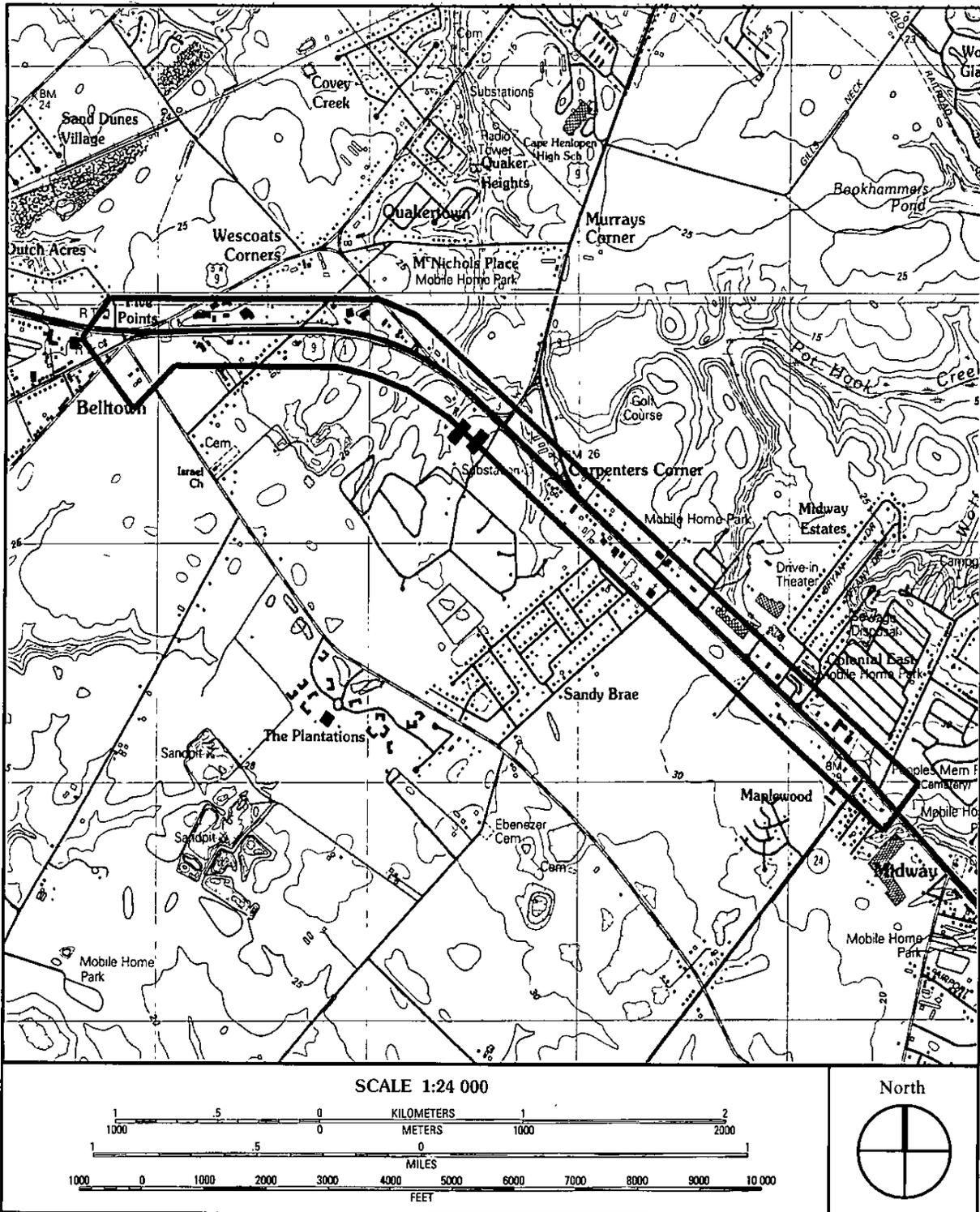
**Plate 8.** View northwest along SR 1 from Ratcliffe Drive.



**Plate 9.** View southeast along SR 1 from Brian Drive.



**Plate 10.** View northwest along SR 1 from Wolf Neck Road.



**Figure 2.** Architectural Area of Potential Effects. (Source, the USGS Lewes, Del. and Farimount, Del. Quadrangle, 1984).

The *Delaware Comprehensive Historic Preservation Plan* identifies the following preservation priorities that may apply to historic architectural, or above-ground, resources in the survey:

1. Priority Historic Themes for Above-Ground Resources:  
Agriculture (#1)  
Transportation and Communication (#5)  
Retailing/Wholesaling (#7)  
Settlement Patterns and Demographic Change (#11)  
Architecture (#12)  
Major Families (#18)
2. Priority Chronological Periods for Above-Ground Resources:  
1830-1880 +/- Industrialization and Early Urbanization (#2)  
1880-1940 +/- Urbanization and Early Suburbanization (#3)
3. Priority Geographic Zones for Above-Ground Resources:  
Lower Peninsula/Cypress Swamp (#3)

*Report and Survey Form Preparation*

All findings and analyses described above were documented in writing and assembled in this draft report according to requirements presented in the *Delaware Comprehensive Historic Preservation Plan*. Maps and photographs are included as Figures and Plates, respectively.

This preliminary report is being submitted for review to WRA, DelDOT, and the Delaware State Historic Preservation Office (DE SHPO) with the understanding that upon the receipt of comments, the KSK will prepare the final report.

Background Research

### III. BACKGROUND RESEARCH

#### Geographic Setting

The Project Area (PA) is located along SR 1/Route 9 from Five Points to SR 24 in Lewes and Rehoboth Hundred, Sussex County, Delaware. Proposed improvements include adding an additional travel lane on the west side of the existing roadway. The Area of Potential Effects (APE) was defined as properties adjacent to these roads, properties with direct views into the PA, or areas of proposed ground disturbance.

Sussex County is Delaware's southernmost county, situated at the confluence of the Delaware River and the Atlantic Ocean. It is also the largest at 950 square miles. Historically it contained approximately one-quarter of the state's population spread over 600,000 acres, a density of 20 acres per person. Primarily an agricultural community, it is also known for its beach resorts. Seaford, home of a Du Pont company plant, is one of its few industrial centers. The county seat is Georgetown in central Sussex.

T. Coleman du Pont built SR 1 in 1917-19. Originally called the Du Pont Boulevard it crossed Sussex County from Rehoboth to Milford, connecting and paving a series of disjointed local roads. The portion between Carpenters Corner and Five Points was a new road, built at that time across open farmland and marsh.

Five Points, though a historic intersection, has a modern name, designating the community surrounding the new intersection of Business Route 9, SR 1/Route 9 (Coastal Highway), SR 23, and Beaver Dam Road. It lies southwest of Lewes, just south of the former Junction and Breakwater Railroad, and east of the Belltown Historic District.

The area east of Five Points was originally called Prettymanville, named after a cluster of Prettyman farms. The family also owned the local store. The word "prettyman" is Scottish, meaning a stout warrior.<sup>1</sup> Lewes, Prettymanville and Rehoboth Beach were the only communities noted on De Beers' 1868 atlas for Lewes and Rehoboth Hundred.

Belltown was a "free colored" community founded circa 1840 by Jacob Bell just west of Prettymanville. Under Delaware law, "colored" referred to people more than one-sixteenth non-European, which included, as in Bell's case, Native American ancestry. Belltown is located between Route 9 and SR 23 just south of SR 1 and west of Prettymanville. Though not listed on the 1868 atlas, Belltown was a distinct community and is potentially eligible for the National Register of Historic Places as a district. Bell's house was listed in the National Register in 1982.

#### Cultural History

The prehistoric and historic period of this region has been extensively studied and reported. Specifically, this area has been fully documented in a previous cultural resources report. Consequently, KSK has not duplicated this material in this report. We have included general background history for the nineteenth and twentieth centuries as context for evaluating resources since the project area contains cultural resources from these periods.

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<sup>1</sup> Sir Walter Scott. *Waverly* (London, 1929).

*Nineteenth Century*

Delaware's economy was stimulated by the Embargo Acts preceding the War of 1812, which greatly diminished the supply of European manufactured goods to America. By 1810, Delaware had four paper mills, five forges, three cotton and two woolen mills, several iron rolling and slitting mills. A notable innovator, Eleuthère Irénée du Pont, founded a gunpowder mill, and the du Pont dynasty, at Hengley in 1803. The Eleutherian Mills produced greatly superior and more reliable powder than ever before manufactured in America, revolutionizing American warfare.

Hagley

Du Pont was one of the few manufacturers to survive after 1815, when British goods again flooded the domestic market. Delaware's main industry reverted to wheat production, with most of its other business activity ancillary to farming. Into the 1900s, most of its roads, landings, canneries, rail spurs, warehouses, and mills were constructed to support an economy and culture predicated on agriculture.<sup>2</sup>

Farmland was most valuable in New Castle County. Each acre was valued at approximately seventy dollars, with the percentage of improved acreage approximately eighty-two percent. Kent County farms were significantly lower in value, at twenty-eight dollars per acre. The percentage of improved land on each farm was also significantly lower, thirty-three percent. In Sussex County, almost one-half of the land of each farm was unimproved, and each acre valued at approximately thirteen dollars. These statistics reflect the remoteness of Sussex County from developing urban markets and efficient overland transportation routes, the frequency of marshland and watercourses, and the subsequent extremely low density of the population in Sussex County (Figure 3).

The bulk of Delaware's agricultural laborers were originally slaves. The Dutch West Indies Company, the largest European slave dealer and owner of New Netherlands, introduced the first slaves to Delaware in 1639. By 1664, slaves comprised 20% of the population, continuing to rise as English colonists emigrated from Maryland and brought slaves with them. Not until the 1800s would the percentage of slave to free black population began to shift, a trend attributed to the growth of Methodism. In 1790, 95% of the African population in Delaware were enslaved. By 1860 there were 90,000 whites, 20,000 free blacks, and only 1,800 slaves.<sup>3</sup>

Delaware became a border state during the Civil War, with New Castle County pro-Union and Sussex County pro-slavery. Approximately 12,000 men joined the Union Army and only 500 joined the Confederacy. The Union had a field hospital in Wilmington and a POW camp at Fort Delaware on Pea Patch Island. Wilmington was the center of Delaware's war effort, manufacturing steam ships, wagons, shoes, ammunition containers and holsters for the north.<sup>4</sup>

the divide did not follow county lines that neatly

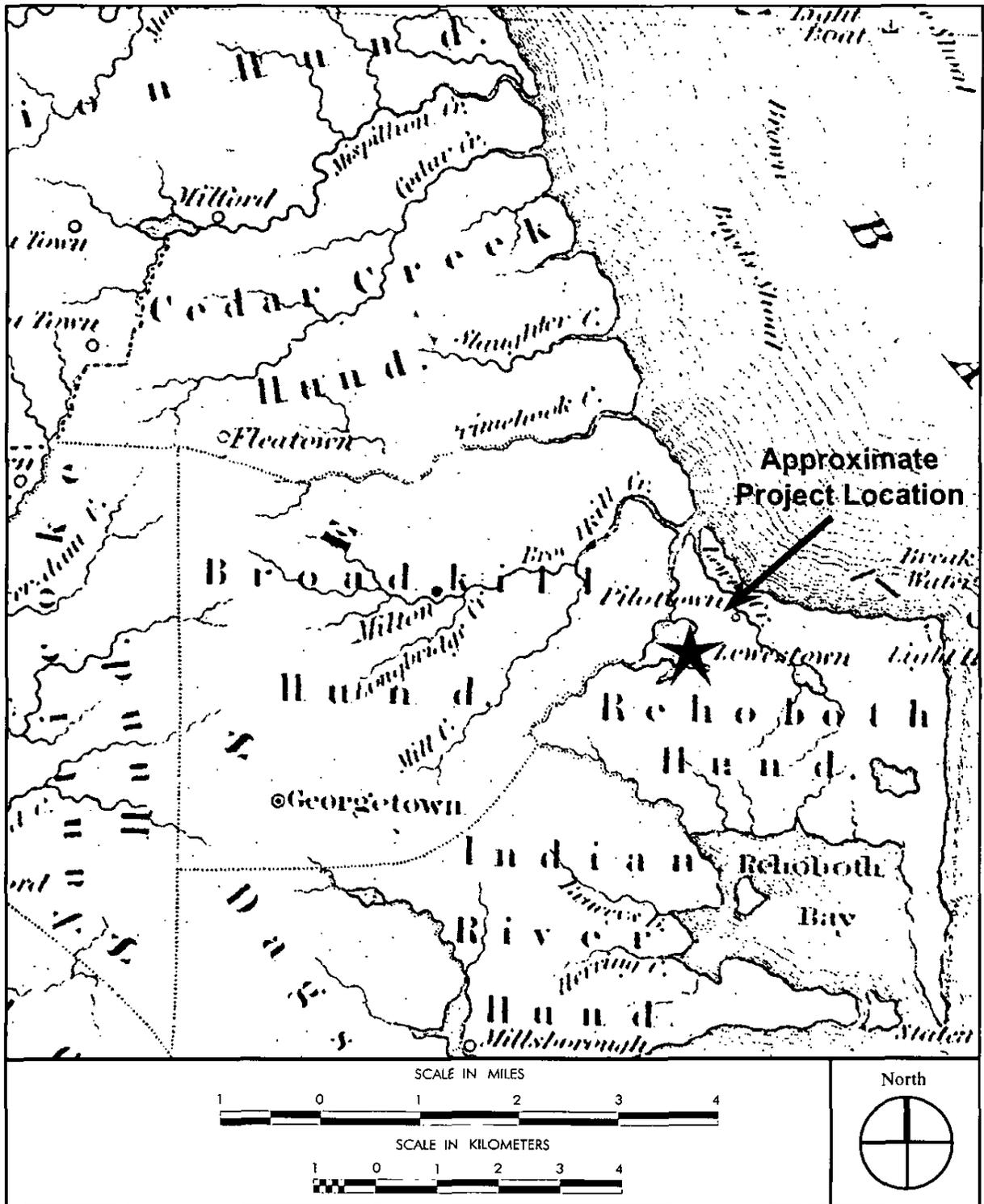
Historically most of Delaware's manufacturers, and farmers, transported their products by water. Many Delaware rivers have access to the Atlantic Ocean, including the Christina, Mispillion, Broadkill, Saint Jones and Murderkill. Towns along these rivers became shipbuilders and transportation hubs. Milford and Milton each had three shipyards and Seaford one.<sup>5</sup> Frederica retained its shipbuilding industry into the 1890s, and regular steamer service served the town until

<sup>2</sup> John A. Monroe. *History of Delaware* (Newark, DE: The University of Delaware Press, 1993), 103-107.

<sup>3</sup> Hoffecker, 135.

<sup>4</sup> Hoffecker, 140-47.

<sup>5</sup> Harold B. Hancock. *The History of Sussex County, DE* (Georgetown, DE: Sussex County Bicentennial Committee, 1976), 90.



**Figure 3.** Sussex County c. 1838. (Source, G. W. Boynton's *The First Two Railroads*, Bradford & Goodrich, 1838).

1929. Most ports specialized in local produce. Drawbridge, for example, shipped grain, timber and muskrats to Philadelphia, and Nassau was a port for milk and fruit.<sup>6</sup>

With such an abundance of navigable rivers, most of Delaware's roads were poor-quality and provided only local access. Capital, labor, and engineering skills were limited in rural communities, and bad roads along short distances did not greatly impinge on the movement of goods to market.<sup>7</sup> One of the few through routes was the King's Highway connecting New Castle and Lewes, the two most important places in Delaware in the early 19<sup>th</sup> century. Originally a crooked and sandy succession of country roads, it was eventually lengthened and straightened into the continuous State Road from Wilmington into Maryland, with branches to Lewes and Seaford.<sup>8</sup> (Figure 4)

Overland travel was slightly improved with the invention of turnpikes. Turnpikes were paved toll roads built, usually by non-local investors, for the transportation of goods along overland routes where these routes were competitive in price or travel time with waterborne transportation. The Newport and Gap, Delaware's first turnpike and first road to be improved with crushed stone, was completed in 1808.<sup>9</sup> Built over an old Indian trail, it connected southern Delaware with Christiana in Pennsylvania.<sup>10</sup> By the mid-nineteenth-century, many of Delaware's ports had turnpikes radiating out into the farmland of Delaware and Pennsylvania.<sup>11</sup> Turnpikes, however, continued to remain ancillary to shipping until the invention of the automobile.

Contemporary with the construction of turnpikes, canals were introduced to augment and improve Delaware's water traffic. Relatively flat topography and abundant water sources made Delaware ideal for canals. The highly successful Chesapeake and Delaware Canal was built in 1824-29. It connected the Delaware River with Back Creek on the Chesapeake Bay. A tidal canal 13.6 miles in length and deepened over time to 27 feet deep, it shortens the route from Baltimore to Philadelphia by 316 miles, New York by 179 miles and Europe by 100 miles.<sup>12</sup> Owned and operated by the federal government, toll-free, since 1919, it is one of the few canals still in active service.<sup>13</sup>

Unlike most states, the introduction of railroads in Delaware in the mid-1800s did not replace canals. Instead, it decreased shipping along Delaware's navigable rivers. Shipbuilding and water transport had peaked during the Civil War and Reconstruction, but by 1887 began to cease altogether.<sup>14</sup> As marine railroads replaced river traffic, trade was withdrawn from the tidewater landings, and new villages created inland, including Harrington, Ellendale, Viola, and Houston.<sup>15</sup>

Delaware had many railroad lines, both passenger and freight, beginning in 1831 with the New Castle and Frenchtown (NC&F) line between New Castle, DE and Frenchtown, MD. 161.9 miles

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<sup>6</sup> WPA, 375, 407.

<sup>7</sup> Taylor, George Rogers. "The Transportation Revolution: 1815-60." *The Economic History of the United States, Vol. IV* (White Plains, NY: M. E. Sharpe, Inc., 1951), 16.

<sup>8</sup> WPA, 73, 403.

<sup>9</sup> WPA, 445-6

<sup>10</sup> C.A. Weslager. *Delaware's Buried Past* (New Brunswick, NJ: Rutgers University Press, 1968), 144

<sup>11</sup> Beers, D. G. *Atlas of the State of Delaware*. (Philadelphia: Pomeroy & Beers, 1868).

<sup>12</sup> Hoffecker, 120.

<sup>13</sup> WPA, 335.

<sup>14</sup> George A. Harter. *Dodge's Geography of Delaware* (Rand McNally & Co., 1911), 13.

<sup>15</sup> WPA, 77.



**Figure 4.** Lewes and Rehoboth Hundred c. 1850. (Source, Jacob Price and Samuel Rea's *Map of the State of Delaware*, Robert E. Smith, 1850).

long, it comprised the rail link in a water-rail-water route from Philadelphia to Baltimore.<sup>16</sup> Local lines were built connecting the NC&F to smaller ports on the Atlantic Ocean and Chesapeake Bay, such as the Junction and Breakwater Railroad from Harrington to the port of Lewes completed in 1868.<sup>17</sup> By the late 1880s the Philadelphia, Wilmington & Baltimore RR had linked together a through line from Newark DE to Cape Charles, VA with branches to Smyrna and Rehoboth Beach. Rehoboth Beach, Dewey Beach, Bethany Beach and Fenwick Island resorts were created when dedicated passenger service extended to the coast at the turn of the century.<sup>18</sup> (Figure 5)

Railroads with their refrigerated cars and rapid speeds introduced new crops to Delaware, such as blackberries, apples, tomatoes, and strawberries. They particularly helped farmers capitalize on the Peach Boom of 1840-80, when scientists discovered that grafting young trees produced superior and abundant fruit. It was during this period that Major Philip Reybold, the Peach King of Delaware, became the richest farmer in the history of Delaware.<sup>19</sup>

### *Twentieth Century*

The development of a paved road network through Delaware faltered from 1903 through 1917.<sup>20</sup> A State Aid Law appropriating funds for road improvement passed in 1903, but was repealed in 1905.<sup>21</sup> In 1909, the legislature nearly passed a bill to pave the road from Wilmington to Georgetown, but did not.<sup>22</sup> Even the efforts of T. Coleman du Pont, who offered to fund construction of this same alignment, were stymied from 1911 to 1915 in court. After reaching the United States Supreme Court to resolve eminent domain powers, construction of this road proceeded, and, in 1917, a twenty-mile section in Sussex County was dedicated (Figure 6).

Coleman du Pont donated \$4,000,000 to the Boulevard, one of many state projects the du Pont family would sponsor. In 1930, Henry Francis du Pont established the Winterthur Corporation, a non-profit, educational organization and museum on his estate in Wilmington. Throughout the early 20<sup>th</sup> century, Pierre S. du Pont replaced existing one-room schoolhouses statewide with modern buildings equipped with auditoriums, gymnasiums, cafeterias and libraries.<sup>23</sup>

Encouraged by the success of the Coleman du Pont Boulevard, the Delaware State Highway Department was created, and almost immediately assumed responsibility for construction of the remaining length of the road, from Milford north to Wilmington.<sup>24</sup> The du Pont was a sixteen-foot wide concrete road, within a sixty-five foot right-of-way, that overlaid previously existing roads. For the first time, rural Delaware was connected with the major urban markets that had previously been accessible only by rail or water.<sup>25</sup>

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<sup>16</sup> Hank Mummert, ed. *Hobo's Guide to the Delmarva Branch*. <http://kc.pennsyrr.com/guide/delmarva.html>.

<sup>17</sup> Hancock, 64.

<sup>18</sup> Hoffecker, 160.

<sup>19</sup> WPA, 468.

<sup>20</sup> The bicycling enthusiast's "Good Roads" efforts, often attributed with the initial impetus for road improvement, appears to have made no headway in southern Delaware. H. Clay Reed. *Delaware: A History of the First State* (New York: Lewis Historical Publishing Co., 1947), 537. Seely, 11.

<sup>21</sup> Reed, 538.

<sup>22</sup> WPA, 79.

<sup>23</sup> Hoffecker, 174.

<sup>24</sup> Reed 545-6.

<sup>25</sup> WPA, 403.



Figure 5. Lewes and Rehoboth Beach Hundred, 1868. (Source, D.G. Beers' *Atlas of the State of Delaware*, Pomeroy & Beers, 1868).



**Figure 6.** SR1 from Five Points to Midway c. 1926. On file at the University of Delaware Spatial Lab (<http://bluhen.ags.udel.edu/spatialab>).

The automobile and improved road technology finally eclipsed waterways as an efficient means of transport, revitalizing Delaware agriculture and in time made even railroads obsolete. Historically, transportation innovations did not reach out into rural areas, and farmers had always faced the difficulty of getting their goods to a distribution point. Continued neglect of local infrastructure had by the early 20<sup>th</sup> century silted up tidewater creeks and ruined already marginal roads. According to the Works Project Administration, "The problem of hauling had so completely baffled many owners and tenants" that production began to drop to subsistence farming.<sup>26</sup> The use of trucks provided the first direct access from farm to market, and agriculture production rose again. A new industry, seasonal canneries, sprang up in almost every town in southern Delaware. New "truck crops" were created, including broiler chickens, which have become a staple cash crop in Delaware today.<sup>27</sup> By the end of 1998 close to 1.6 billion broilers were being produced yearly in Sussex County.<sup>28</sup>

The emergence of the automobile also led to early and mid-twentieth century residential roadside subdivisions. Historically, land bordering the corridors between population nodes, such as state and country highways, consisted of large-acre agricultural parcels and estates. With the widespread use of automobiles, they became available for the first time for suburban development, and were heavily pursued by developers (Figures 7-8).

Speculative subdivisions occupied only the frontage along a road, instead of the whole parcel. These developments share common physical characteristics. The dwellings are usually built at the same time, and generally exhibit uniformity in size, height, massing, and architectural style. They are built close to the road and share a similar setback. The parcels are also similar in size or property width, and are part of a roadside strip not associated with a larger property. This conformity of building type, size and siting differentiates speculative developments from agricultural properties subdivided to accommodate successive generations.<sup>29</sup>

A predominate house type has developed along these roads, as identified by Herman and Siders in *Historic Context Master Reference and Summary*. The two-story, hall and parlor and center-passage farmhouse of the nineteenth century has given way to one and one-half story, single pile, and three or five bay bungalows. Both types, however, are built without cellars or full foundations and are therefore easily, and frequently, moved as commercial and transportation developments encroach on right-of-ways.<sup>30</sup>

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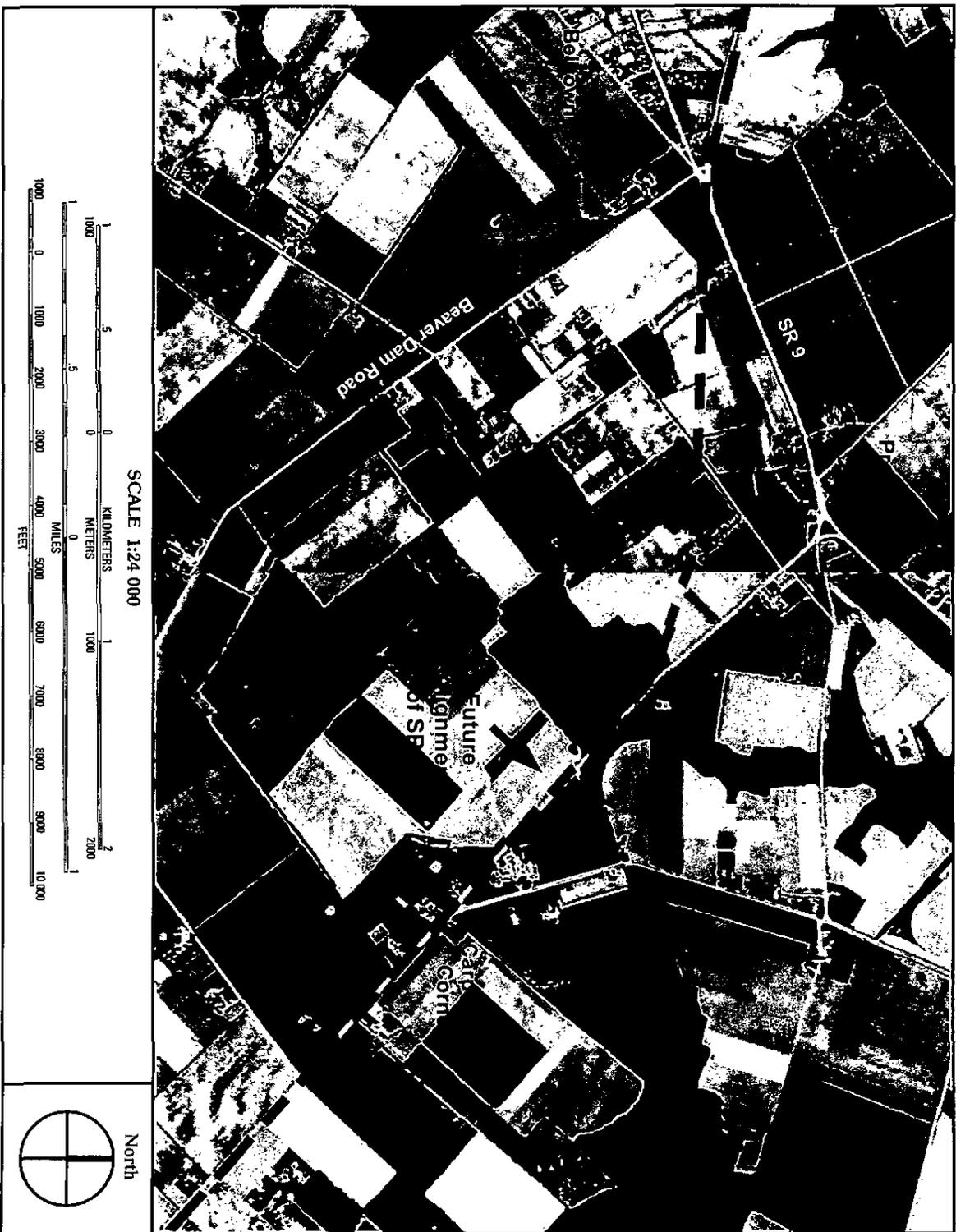
<sup>26</sup> WPA, 79.

<sup>27</sup> Hoffecker, 158-172.

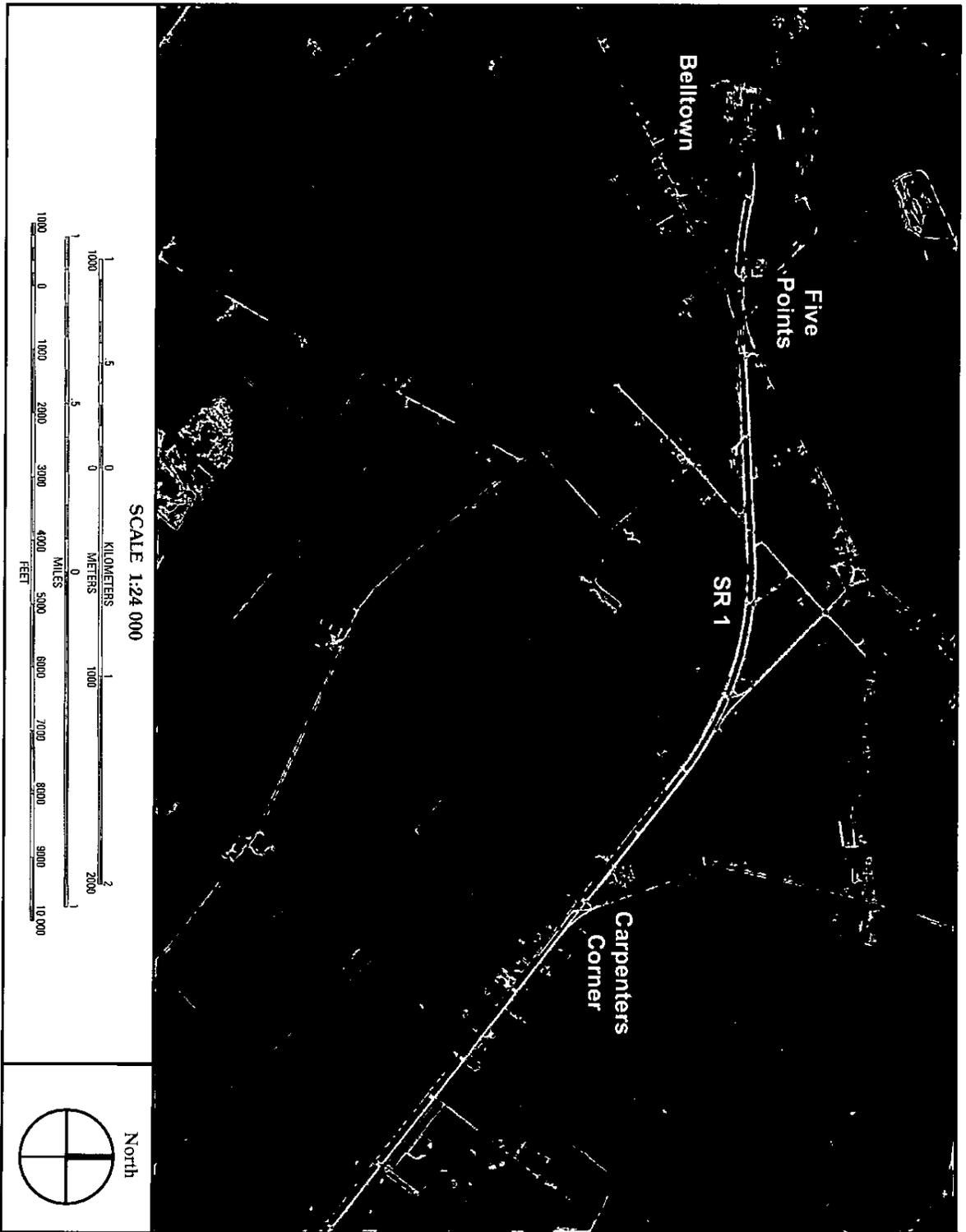
<sup>28</sup> Sussex County Homepage. <http://www.co.sussex.de.us>

<sup>29</sup> Kenneth T. Jackson. *Crabgrass Frontier* (New York: Oxford University Press, 1985), 157-189

<sup>30</sup> Bernard Herman and Rebecca J. Siders, et al. *Historic Context Master Reference and Summary*. (Newark, DE: University of Delaware Center for Historic Architecture and Engineering, 1989), 93-96.



**Figure 7.** SR1 from Five Points to Midway c. 1938. On file at the University of Delaware Spatial Lab (<http://bluehen.ag.s.udel.edu/spatialab>).



**Figure 8.** SR1 from Five Points to Midway c. 1960. On file at the University of Delaware Spatial Lab (<http://blueten.ags.udel.edu/spatlab>).

Description of Work

IV

#### IV. DESCRIPTION OF WORK

##### *Archaeological Resources*

A Phase I archaeological survey that encompassed the project area and included extensive shovel testing was conducted by John Milner Associates, Inc. (JMA) between March 1987 and April 1988 as detailed in DelDOT's Architectural and Cultural Series No. 79 (1990). No prehistoric or historic sites or deposits were identified during JMA's survey.

KSK conducted a pedestrian survey across the entire project area to document previous ground disturbance. Approximately seventy percent of the project area is heavily impacted by recent commercial and retail development.

The project area tested by JMA extended approximately 25 feet from the edge of the existing roadway, which at that time was approximately 100 feet wide. The proposed road improvements described above will occur entirely within the area previously tested by JMA. Consequently, unless the area for potential ground disturbance changes it is KSK's opinion that no additional archaeological work is warranted.

##### *Historical Architectural Resources*

The study revealed seventeen buildings within the Area of Potential Effects (APE) fifty years of age or older. JMA surveyed this area as part of a larger cultural resources investigation in 1988. JMA made the determination that three of the above-referenced seventeen buildings appeared eligible for the National Register and that the other fourteen resources did not meet National Register criteria for significance or that the resources lacked integrity and could no longer convey significance. A number of significant alterations have occurred to the previously surveyed properties and specifically to the three properties previously determined eligible by JMA. These alterations, in KSK's opinion, have resulted in a loss of integrity to these three properties. Therefore, in KSK's opinion none of seventeen resources are eligible for the National Register of Historic Places. KSK did not identify any additional resources within the APE.

\*S-954

Table 1. Architectural Resources Identified within the Area of Potential Effects.

TAX PARCEL #	NAME/ ADDRESS	CRS #	CONSTRUCTION DATE	NATIONAL REGISTER ELIGIBILITY
3-34-5-222	Knapp Farmstead	S-921	c. 1860	Ineligible - integrity
3-34-6-498	1310 Coastal Road	S-8059	c. 1935	Ineligible - significance
3-34-6-497	1408 Coastal Road	S-8058	c. 1925	Ineligible - significance
3-34-6-495	1410 Coastal (Crab Restaurant)	S-984	c. 1945	Ineligible - integrity
3-34-6-493 (1)	Davidson House	S-985	c. 1915	Ineligible - integrity
3-34-6-493 (2)	1414 Coastal Road (Antique Shop)	S-985	c. 1925	Ineligible - integrity
3-34-6-489	1520 Coastal Road	S-987	c. 1925	Ineligible - significance
3-34-6-488	1532 Coastal Road	S-8057	c. 1935	Ineligible - significance
3-34-6-482	1617 Coastal Road (Bait and Tackle)	S-8056	c. 1935	Ineligible - significance
3-34-6-479	2021 Coastal Road (Funeral Home)	S-989	c. 1935	Ineligible - significance
3-34-12-93	The Seafood Restaurant First Union Bank	S-8016	1922	Ineligible - integrity
3-34-6-80	1565 Coastal Road	S-8052	c. 1945	Ineligible - significance
3-34-6-77	1548 Coastal Road	S-8051	c. 1945	Ineligible - significance
3-34-6-74	1683 Coastal Road	S-8050	c. 1935	Ineligible - significance
3-34-6-72	Givens House	S-8049	c. 1935	Ineligible - significance
3-34-6-71	Beaman House	S-986	c. 1925	Ineligible - significance
3-34-6-64	Carpenter Farmstead	S-1041	c. 1880	Ineligible - integrity

**Resource Description and Evaluation**

Descriptions and evaluations of the surveyed architectural resources are presented below and photographs are included in the Plates section of the report.

*Knapp Family Farmstead, c. 1860*  
Tax No. 3-34-5-222  
CRS No. S-921

- **Assessment**  
This property was previously surveyed by John Milner & Associates (JMA) in 1988, as documented in DelDOT's Architectural and Cultural Series No. 79, *Cultural Resources Survey Delaware Route 1: Five Points to Rehoboth* (1990). At that time, JMA determined the property to be eligible for the National Register of Historic Places under Criterion A as "representative of a typical farm dating from the mid-to-late-nineteenth century." Sometime after 1994 the majority of farm buildings, including the barns and corncrib have been either fallen down or been demolished. The property, therefore, is no longer representative of a mid-to-late nineteenth century farmstead. Individually, the farmhouse is not distinctive for its architecture and has been altered with the application of aluminum/vinyl siding (Plates 11-13).

In KSK's opinion the property no longer retains integrity as an agricultural complex, and therefore, can not be considered eligible for the National Register.



**Plate 11.** View south to the abandoned Knapp Farmstead.



**Plate 12.** View northwest to the abandoned Knapp Farmstead



**Plate 13.** View west to demolished outbuildings associated with the Knapp Farmstead.

- Delaware Historic Context  
 Geographic Zone: Lower Peninsula/Cypress Swamp  
 Chronological Period: 1830-1880 +/- industrialization and early urbanization  
 Historic Theme: Agriculture  
 Property Type: Architecture; I-House

*1310 Coastal Road (Tzovaras House), c. 1935*

Tax No. 3-34-6-498

CRS No. S-8059

- Assessment  
 1310 Coastal Road was previously surveyed by John Milner & Associates (JMA) in 1988, as documented in DelDOT's Architectural and Cultural Series No. 79, *Cultural Resources Survey Delaware Route 1: Five Points to Rehoboth* (1990). JMA concluded that this house was typical of its period, lacked architectural distinction, and would not meet National Register criteria. We concur with both JMA's assessment of the property and their recommendation that the property is ineligible for the National Register of Historic Places due to a lack of significance (Plates 14-15).
- Delaware Historic Context  
 Geographic Zone: Lower Peninsula/Cypress Swamp  
 Chronological Period: 1880-1934+/- Urbanization and Early Suburbanization  
 Historic Theme: Architecture  
 Property Type: Architecture; Cape Cod

*1408 Coastal Road (Texas Highway One House), c. 1925*

Tax No. 3-34-6-497

CRS No. S-8058

- Assessment  
 1408 Coastal Road was previously surveyed by John Milner & Associates (JMA) in 1988, as documented in DelDOT's Architectural and Cultural Series No. 79, *Cultural Resources Survey Delaware Route 1: Five Points to Rehoboth* (1990). JMA concluded that this property was architecturally undistinguished and did not meet National Register criteria. We concur with both JMA's assessment of the property and their recommendation that the property is ineligible for the National Register of Historic Places due to a lack of significance (Plates 16-17).
- Delaware Historic Context  
 Geographic Zone: Lower Peninsula/Cypress Swamp  
 Chronological Period: 1880-1934+/- Urbanization and Early Suburbanization  
 Historic Theme: Architecture  
 Property Type: Architecture; bungalow



Plate 14. View south to 1310 Coastal Road.

- WRONG PHOTOS



Plate 15. View west to 1310 Coastal Road.



**Plate 16.** View south to 1408 Coastal Road.



**Plate 17.** View west to 1408 Coastal Road.

*1410 Coastal Road (Crab Restaurant), c. 1945*

Tax No. 3-34-6-495

CRS No. S-984

- Assessment  
1410 Coastal Road was previously surveyed by John Milner & Associates (JMA) in 1988, as documented in DelDOT's Architectural and Cultural Series No. 79, *Cultural Resources Survey Delaware Route 1: Five Points to Rehoboth* (1990). Formerly a single-family dwelling, a large one-story, three bay, front gable addition has been added to the front of the original dwelling more than doubling its size. The building was converted into a seafood restaurant following the construction of the addition. JMA concluded that the original dwelling was not a distinctive example of its style. The subsequent addition has completely obscured the original dwelling. In KSK's opinion this building is not eligible for the National Register (Plates 18-19).
- Delaware Historic Context  
Geographic Zone: Lower Peninsula/Cypress Swamp  
Chronological Period: 1880-1934+/- Urbanization and Early Suburbanization  
Historic Theme: Architecture  
Property Type: Architecture; International Style

*Davidson House, c. 1915*

Tax No. 3-34-6-493 (1)

CRS No. S-985

- Assessment  
The Davidson House was previously surveyed by John Milner & Associates (JMA) in 1988, as documented in DelDOT's Architectural and Cultural Series No. 79, *Cultural Resources Survey Delaware Route 1: Five Points to Rehoboth* (1990). JMA concluded that the Davidson House lacked integrity, and therefore, could not be considered for National Register eligibility. We concur with both JMA's assessment of the property and their recommendation that the building is ineligible for the National Register of Historic Places (Plates 20-21).
- Delaware Historic Context  
Geographic Zone: Lower Peninsula/Cypress Swamp  
Chronological Period: 1880-1934+/- Urbanization and Early Suburbanization  
Historic Theme: Architecture  
Property Type: Architecture; Four Square

*1414 Coastal Road (Antique Shop), c. 1925*

Tax No. 3-34-6-493 (2)

CRS No. S-985

- Assessment  
1414 Coastal Road was previously surveyed by John Milner & Associates (JMA) in 1988, as documented in DelDOT's Architectural and Cultural Series No. 79, *Cultural Resources Survey Delaware Route 1: Five Points to Rehoboth* (1990). Formerly a single-family dwelling, the building has been converted into an antique store. The building's entry vestibule was altered to include floor to ceiling glass walls. JMA concluded that this house was not a distinguished example of its type and did not meet National Register criteria. We concur with



**Plate 18.** View south to 1410 Coastal Road showing transformation to a restaurant.



**Plate 19.** View southwest to 1410 Coastal showing new front addition.



**Plate 20.** View southwest to the Davidson House (lawyers office) formerly a residential dwelling.



**Plate 21.** View southwest to the outbuilding associated with the Davidson House.

JMA's recommendation, and subsequent alterations to the building have further compromised the its integrity. In KSK's opinion the property is not eligible for the National Register (Plates 22-23).

- Delaware Historic Context  
Geographic Zone: Lower Peninsula/Cypress Swamp  
Chronological Period: 1880-1934+/- Urbanization and Early Suburbanization  
Historic Theme: Architecture; residence  
Property Type: Architecture; bungalow

*1520 Coastal Road (Foraker House), c. 1925*

Tax No. 3-34-6-489

CRS No. S-987

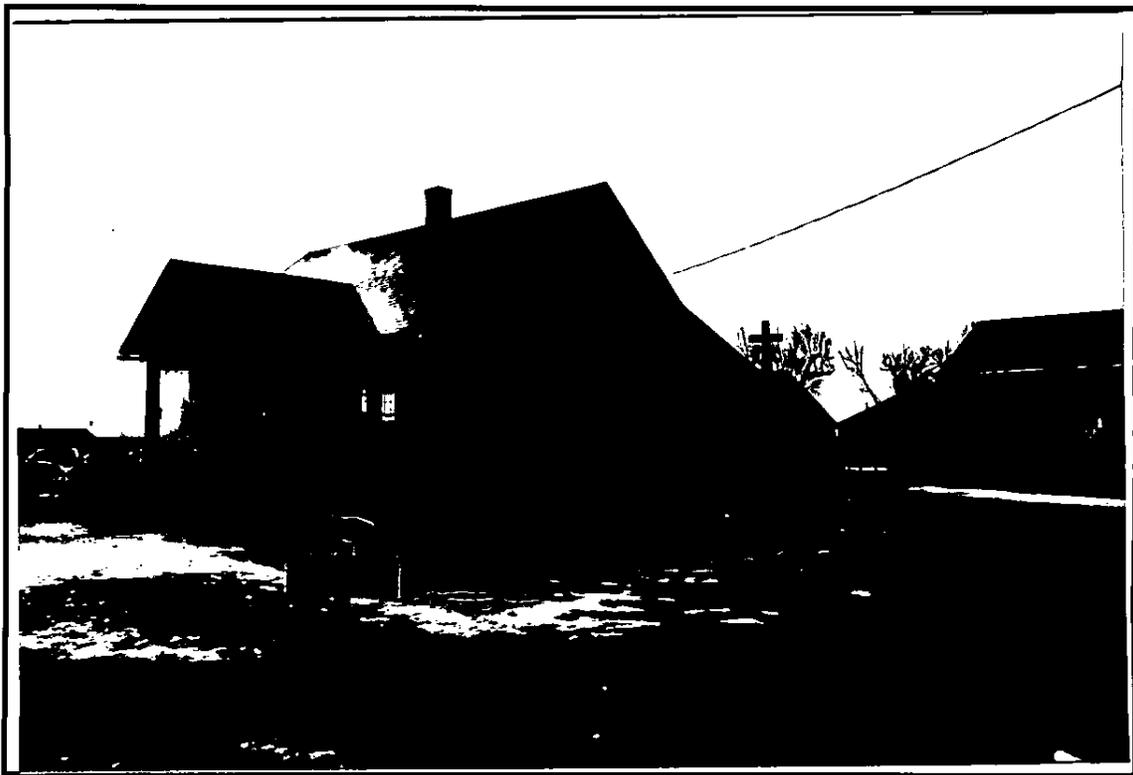
- Assessment  
1520 Coastal Road was previously surveyed by John Milner & Associates (JMA) in 1988, as documented in DelDOT's Architectural and Cultural Series No. 79, *Cultural Resources Survey Delaware Route 1: Five Points to Rehoboth* (1990). JMA concluded that the house lacked architectural merit and would not qualify for inclusion in the National Register. We concur with both JMA's assessment of the property and their recommendation that the property is ineligible for the National Register of Historic Places due to a lack of significance (Plates 24-25).
- Delaware Historic Context  
Geographic Zone: Lower Peninsula/Cypress Swamp  
Chronological Period: 1880-1934+/- Urbanization and Early Suburbanization  
Historic Theme: Architecture  
Property Type: Architecture; bungalow

*1532 Coastal Road (Horn House), c. 1935*

Tax No. 3-34-6-488

CRS No. S-8057

- Assessment  
1520 Coastal Road was previously surveyed by John Milner & Associates (JMA) in 1988, as documented in DelDOT's Architectural and Cultural Series No. 79, *Cultural Resources Survey Delaware Route 1: Five Points to Rehoboth* (1990). JMA concluded that this house was not unique or distinctive and did not meet National Register criteria. We concur with both JMA's assessment of the property and their recommendation that the property is ineligible for the National Register of Historic Places due to a lack of significance (Plates 26-27).
- Delaware Historic Context  
Geographic Zone: Lower Peninsula/Cypress Swamp  
Chronological Period: 1880-1934+/- Urbanization and Early Suburbanization  
Historic Theme: Architecture  
Property Type: Architecture; Cape Cod



**Plate 22.** View south to 1414 Coastal Road (antique shop) formerly a single-family dwelling.



**Plate 23.** View west to 1414 Coastal Road.



**Plate 24.** View south to 1520 Coastal Road.



**Plate 25.** View west to 1520 Coastal Road.



**Plate 26.** View south to 1532 Coastal Road.



**Plate 27.** View west to 1532 Coastal Road.

*1617 Coastal Road (Bait and Tackle/Beauty Shop), c. 1935*

Tax No. 3-34-6-482

CRS No. S-8056

- Assessment  
1617 Coastal Road was previously surveyed by John Milner & Associates (JMA) in 1988, as documented in DelDOT's Architectural and Cultural Series No. 79, *Cultural Resources Survey Delaware Route 1: Five Points to Rehoboth* (1990). JMA concluded that this building was a typical example of Colonial Revival style architecture applied to a suburban residence, but was not a distinctive example of its type. This building was converted from a single-family dwelling into a bait and tackle and beauty supply store. A screened porch was converted into an enclosed storage space at that date. In KSK's opinion the building is not eligible for the National Register (Plates 28-29).
- Delaware Historic Context  
Geographic Zone: Lower Peninsula/Cypress Swamp  
Chronological Period: 1880-1934+/- Urbanization and Early Suburbanization  
Historic Theme: Architecture, residence  
Property Type: Architecture; Cape Cod

*2021 Coastal Road (Funeral Home), c. 1935*

Tax No. 3-34-6-479

CRS No. S-989

- Assessment  
2021 Coastal Road was previously surveyed by John Milner & Associates (JMA) in 1988, as documented in DelDOT's Architectural and Cultural Series No. 79, *Cultural Resources Survey Delaware Route 1: Five Points to Rehoboth* (1990). JMA concluded that this building did not meet National Register criteria. Alterations were made to the flanking wings of the house further compromising its architectural integrity. We concur with both JMA's assessment of the property and their recommendation that the building is ineligible for the National Register of Historic Places due to a lack of significance and integrity (Plates 30-31).
- Delaware Historic Context  
Geographic Zone: Lower Peninsula/Cypress Swamp  
Chronological Period: 1880-1934+/- Urbanization and Early Suburbanization  
Historic Theme: Architecture  
Property Type: Architecture; Colonial Revival

*The Seafood Restaurant, c. 1922*

Tax No. 3-34-12-93

CRS No. S-8016

- Assessment  
This Seafood Restaurant was previously surveyed by John Milner & Associates (JMA) in 1988, as documented in DelDOT's Architectural and Cultural Series No. 79, *Cultural Resources Survey Delaware Route 1: Five Points to Rehoboth* (1990). At that time, JMA determined the property to be potentially eligible for the National Register of Historic Places under Criterion C as representative of an unusual house type and Criterion B for its association with a prominent family. Since 1990, however, this former single-family dwelling has been converted into a restaurant and a full, one-story, wraparound seating area added to



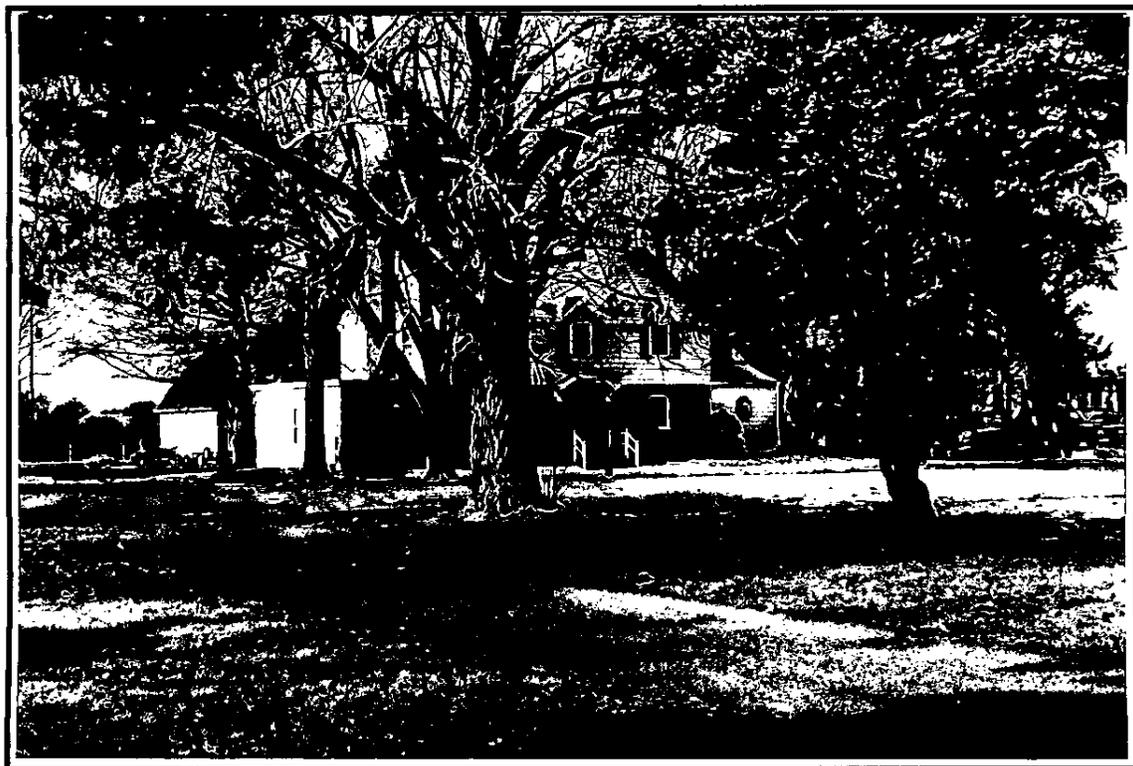
**Plate 28.** View south to 1617 Coastal Road, now a retail shop and salon.



**Plate 29.** View west to 1617 Coastal Road.



**Plate 30.** View south to 2021 Coastal Road (funeral home).



**Plate 31.** View west to 2021 Coastal Road (funeral home)

the building. The addition conceals original details and resulted in the removal of smaller front porch and side port cochere. The present use and additions overwhelms the original dwelling and obscures its associations as a status-symbol house, both important elements to the original evaluation.

In KSK's opinion, the property no longer retains integrity and can no longer convey its significance, and therefore, can not be considered eligible for the National Register (Plates 32-33).

- Delaware Historic Context  
Geographic Zone: Lower Peninsula/Cypress Swamp  
Chronological Period: 1880-1934+/- Urbanization and Early Suburbanization  
Historic Theme: Architecture, Major Families  
Property Type: Architecture; Four Square

*1565 Coastal (Dentino House), c. 1945*

Tax No. 3-34-6-80

CRS No. S-8052

- Assessment  
1565 Coastal Road was previously surveyed by John Milner & Associates (JMA) in 1988, as documented in DelDOT's Architectural and Cultural Series No. 79, *Cultural Resources Survey Delaware Route 1: Five Points to Rehoboth* (1990). JMA concluded that this building was architecturally undistinguished and did not meet National Register criteria. We concur with both JMA's assessment of the property and recommendation that it is ineligible for the National Register of Historic Places due to a lack of significance (Plates 34-35).
- Delaware Historic Context  
Geographic Zone: Lower Peninsula/Cypress Swamp  
Chronological Period: 1880-1934+/- Urbanization and Early Suburbanization  
Historic Theme: Architecture  
Property Type: Architecture; modern bungalow

*1548 Coastal Road (Melson House), c. 1945*

Tax No. 3-34-6-77

CRS No. S-8051

- Assessment  
1548 Coastal Road was previously surveyed by John Milner & Associates (JMA) in 1988, as documented in DelDOT's Architectural and Cultural Series No. 79, *Cultural Resources Survey Delaware Route 1: Five Points to Rehoboth* (1990). JMA concluded that this building was typical of its period and did not meet National Register criteria. We concur with both JMA's assessment of the property and recommendation that it is ineligible for the National Register of Historic Places due to a lack of significance (Plates 36-37).
- Delaware Historic Context  
Geographic Zone: Lower Peninsula/Cypress Swamp  
Chronological Period: 1880-1934+/- Urbanization and Early Suburbanization  
Historic Theme: Architecture  
Property Type: Architecture; bungalow



**Plate 32.** View west to the Seafood Restaurant showing wraparound addition.



**Plate 33.** View south to the Seafood Restaurant, formerly a single-family dwelling.



**Plate 34.** View north to 1565 Coastal Road.



**Plate 35.** View east to 1565 Coastal Road.



**Plate 36.** View north to 1548 Coastal Road.



**Plate 37.** View east to 1548 Coastal Road with related outbuildings.

*1683 Coastal Road (Hennessey House), c. 1935*

Tax No. 3-34-6-74

CRS No. S-8050

- Assessment  
1683 Coastal Road was previously surveyed by John Milner & Associates (JMA) in 1988, as documented in DelDOT's Architectural and Cultural Series No. 79, *Cultural Resources Survey Delaware Route 1: Five Points to Rehoboth* (1990). JMA concluded that this building was a typical Colonial Revival house of the period and did not meet National Register criteria. Subsequent to JMA evaluation, the side porch has been altered with the removal and replacement of porch posts and railings. We concur with both JMA's assessment of the property and recommendation that it is ineligible for the National Register of Historic Places due to a lack of significance (Plates 38-39).
- Delaware Historic Context  
Geographic Zone: Lower Peninsula/Cypress Swamp  
Chronological Period: 1880-1934+/- Urbanization and Early Suburbanization  
Historic Theme: Architecture  
Property Type: Architecture; Colonial Revival

*Givens House, c. 1935*

Tax No. 3-34-6-72

CRS No. S-8049

- Assessment  
The Givens House was previously surveyed by John Milner & Associates (JMA) in 1988, as documented in DelDOT's Architectural and Cultural Series No. 79, *Cultural Resources Survey Delaware Route 1: Five Points to Rehoboth* (1990). JMA concluded that this building was an undistinguished example from its period and did not meet National Register criteria. We concur with both JMA's assessment of the property and recommendation that it is ineligible for the National Register of Historic Places due to a lack of significance and integrity (Plates 40-41).
- Delaware Historic Context  
Geographic Zone: Lower Peninsula/Cypress Swamp  
Chronological Period: 1880-1934+/- Urbanization and Early Suburbanization  
Historic Theme: Architecture  
Property Type: Architecture; Colonial Revival

*Beaman House, c. 1925*

Tax No. 3-34-6-71

CRS No. S-986

- Assessment  
The Beaman House was previously surveyed by John Milner & Associates (JMA) in 1988, as documented in DelDOT's Architectural and Cultural Series No. 79, *Cultural Resources Survey Delaware Route 1: Five Points to Rehoboth* (1990). JMA concluded that this building lacked individual distinction and did not meet National Register criteria. We concur with both JMA's assessment of the property and recommendation that it is ineligible for the National Register of Historic Places due to a lack of significance (Plates 42-43).



**Plate 38.** View north to 1683 Coastal Road.



**Plate 39.** View east to 1683 Coastal Road.



**Plate 40.** View north to the Givens House.



**Plate 41.** View east to the Givens House with related outbuildings.

- Delaware Historic Context  
 Geographic Zone: Lower Peninsula/Cypress Swamp  
 Chronological Period: 1880-1934+/- Urbanization and Early Suburbanization  
 Historic Theme: Architecture  
 Property Type: Architecture; Colonial Revival

*Carpenter Farmstead, c. 1880*

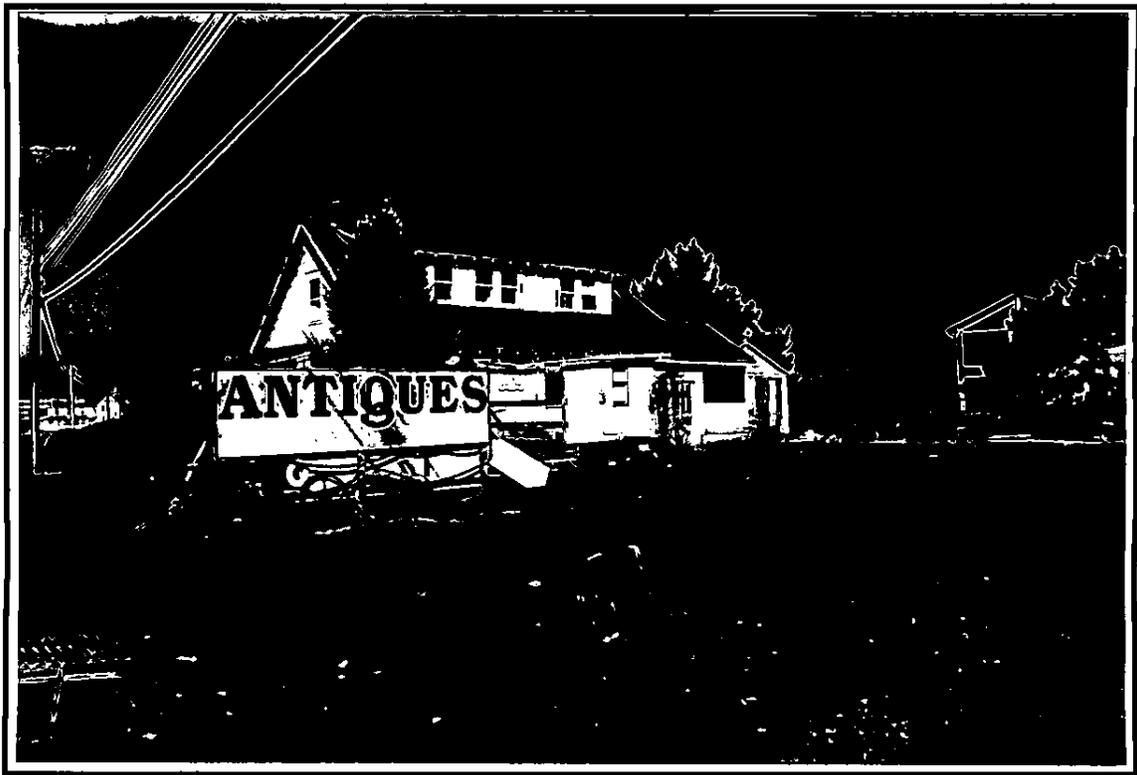
Tax No. 3-34-6-64

CRS No. S-1041

- Assessment  
 This property was previously surveyed by John Milner & Associates (JMA) in 1988, as documented in DelDOT's Architectural and Cultural Series No. 79, *Cultural Resources Survey Delaware Route 1: Five Points to Rehoboth* (1990). At that time, JMA determined the property to be eligible for the National Register of Historic Places under Criterion A as representative of a late-nineteenth/early-twentieth century agricultural complex. Sometime before 1994 all of the agricultural building formerly associated with this farm complex were demolished. The property, therefore, is longer representative of a mid-nineteenth to early twentieth century farm complex. Individually, the farmhouse is not distinctive, is in dilapidated condition, and includes a large enclosed wraparound porch. In KSK's opinion the property no longer retains integrity as an agricultural complex, and therefore, can not be considered eligible for the National Register (Plates 44-45).

It is KSK's recommendation that the property is no longer eligible under Criterion A, and therefore not eligible for the National Register.

- Delaware Historic Context  
 Geographic Zone: Lower Peninsula/Cypress Swamp  
 Chronological Period: 1880-1934+/- Urbanization and Early Suburbanization  
 Historic Theme: Architecture  
 Property Type: Architecture; Southern Colonial



**Plate 42.** View north to the Beaman House (antique shop) formerly a single-family dwelling.



**Plate 43.** View east to the Beaman House.



**Plate 44.** View west to the abandoned Carpenter Farmstead.



**Plate 45.** View northwest to the abandoned Carpenter Farmstead.

Summary

## VI. SUMMARY

As detailed in this report, Kise Straw & Kolodner (KSK) determined that a previously completed cultural resources study addressed both architectural and archaeological resources in the SR 1/Route 9 from Five Points to SR 24 Area of Potential Effects (DelDOT Architectural and Cultural Series No. 79). KSK consulted this report and conducted a survey of the project area. KSK verified that the APE contains seventeen architectural properties that are fifty years of age or older. These same properties were identified in the previous report. Three of the seventeen properties were previously found potentially eligible for the National Register. The other fourteen properties were found not to meet National Register criteria, lacked integrity or contained a combination of both qualities. In KSK's opinion, the three properties previously determined eligible have since lost integrity due to subsequent alterations and additions. KSK, therefore, determined that none of the seventeen properties identified in the APE are eligible for the National Register.

No prehistoric or historic archaeological sites or deposits were identified in the previous survey. The road improvements currently proposed will occur entirely within the area previously tested. Consequently, unless the area for potential ground disturbance changes it is KSK's opinion that no additional archaeological work is warranted.

The proposed improvements associated with the widening of SR 1 between Five Points and SR 24 do not have the potential to affect cultural resources. The APE does not contain any National Register properties; consequently, this report does not include an Assessment of Effects section.

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**Appendix A**

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## VITAE

Judith F. Kennedy serves as a Cultural Resource Specialist and Preservation Planner for Kise Straw & Kolodner. Ms. Kennedy holds an M. S. in Historic Preservation from the University of Pennsylvania and has seven years experience as an architectural historian and cultural resource planner. Her work includes architectural and engineering surveys, Section 106 reviews, preservation and conservation plans, architectural histories, and adaptive reuse planning. She has also served as an archaeologist in the United States and the Middle East. Prior to joining the firm in 2000, Ms. Kennedy owned and operated a full service cultural resources consulting firm. She has worked with both private individuals, and public agencies and institutions including the Delaware, Pennsylvania and New Jersey Departments of Transportation, municipal governments, universities and museums. Ms. Kennedy actively keeps abreast of changing federal preservation statutes, and has recently completed the National Preservation Institute's course on the 2001 Section 106 Regulations.

Douglas B. Mooney earned his Master's Degree in Anthropology from Pennsylvania State University and holds a B.A. in Anthropology from West Virginia University. He also completed two years of post-graduate study at the Institute of Archaeology, University College, London. Principal Archaeologist at Kise Straw & Kolodner Inc., Mr. Mooney has over ten years experience in cultural resources management and has participated in the excavation of more than 100 sites throughout the Mid-Atlantic region and in England. The primary author of more than 25 technical reports and professional papers and presentations, Mr. Mooney's experience encompasses historical, urban, mortuary, and prehistoric archaeology. He is a member of the Society for Pennsylvania Archaeology, the Eastern States Archaeological Federation, the Middle Atlantic Archaeological Conference, and the Philadelphia Archaeological Forum.

Glenn A. Ceponis is an Architectural Historian who graduated from the State University of New York, Buffalo in 1984 with a B.A. in Art History, and has completed two years full time graduate study in historic preservation at the University of Pennsylvania. Mr. Ceponis has also completed "Introduction to Federal Projects and Historic Preservation" offered by the Advisory Council for Historic Preservation. Mr. Ceponis has been employed as an Architectural Historian and Cultural Resources Specialist with Kise Straw & Kolodner (KSK) in their Cultural Resource Group since 1991. Previously, Mr. Ceponis served for nearly three years as Assistant Historian with John Milner Associates, Philadelphia, Pennsylvania. Mr. Ceponis' experience includes conducting documentary research and developing various cultural resource reports, including Section 106 Compliance, Historic American Buildings Survey and Historic American Engineering Record documentation, National Register of Historic Places Nominations, Design Guidelines, and Historic Preservation Certification Applications. He has prepared numerous cultural resources studies for projects located throughout the Mid-Atlantic region and has extensive experience with the New Jersey and Delaware Departments of Transportation, various municipal agencies, the US Army Corps of Engineers, and private developers. Mr. Ceponis currently serves as a Board Member on the Woodbury Historic Preservation Commission in Gloucester County, New Jersey.

Dr. Thomas A.J. Crist is an Associate and Director of Archaeological Services at Kise Straw & Kolodner Inc. He earned his doctorate in Biological Anthropology from Temple University and also holds a Master's degree in Public Service Archaeology from the University of South Carolina. Dr. Crist is nationally recognized as an expert in historical cemetery archaeology and forensic anthropology and currently serves as the Forensic Anthropologist for the Philadelphia Medical Examiner's Office. He has conducted and supervised over 100 archaeological projects

in the United States and Ireland and has extensive experience in Philadelphia historical archaeology. His projects include work at Washington Square, Fort Mifflin, Independence National Historical Park, Merchant's Exchange Building, and the former Blockley Almshouse Burial Ground site. He was certified by the former Society of Professional Archaeologists and is now a Registered Professional Archaeologist (RPA). Dr. Crist has written over 70 professional publications and cultural resources reports, including a recent article for *CRM* magazine entitled *Engaging the Public Through Mortuary Archaeology at Philadelphia's First African Baptist Church Cemeteries*. His second paper on archaeology and public involvement, *Evidence, Empowerment, and Ecology: The Relevance of Mortuary Archaeology to the Public*, is included as a chapter in a National Park Service volume currently in press.

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