
Final Report

**National Register Eligibility Study
Ogletown Road (State Route 273)
Between Marrows Road
and the Amtrak Railroad Lines
White Clay Creek Hundred
New Castle County
Delaware**

Prepared for: Delaware Department of Transportation
Location and Environmental Studies Office
Dover, Delaware

Prepared by: Kise Franks & Straw, Inc.
Historic Preservation Group
Philadelphia, Pennsylvania

April 1995

Final Report

**National Register Eligibility Study
Ogletown Road (State Route 273)
Between Marrows Road
and the Amtrak Railroad Lines
White Clay Creek Hundred
New Castle County
Delaware**

Prepared for: Delaware Department of Transportation
Location and Environmental Studies Office
Dover, Delaware

Prepared by: Kise Franks & Straw, Inc.
Historic Preservation Group
Philadelphia, Pennsylvania

April 1995

• ABSTRACT

Kise Franks & Straw (KFS), of Philadelphia, Pennsylvania, undertook a survey of historic architectural resources within a one-thousand-foot-wide study corridor that extends approximately one mile along State Route 273 (Ogletown Road) between Marrows Road (County Road 351) and the approximate vicinity of the Amtrak Railroad lines in White Clay Creek Hundred, New Castle County, Delaware, during the winter of 1993. KFS conducted the survey on behalf of the Federal Highway Administration and the Delaware Department of Transportation as part of the Section 106 Review process for proposed road improvements along State Route 273. The improvements will result in a four-lane highway separated by a median strip.

KFS identified five resources in the project area that appear to be fifty years of age or older and have the potential to be affected by the proposed improvements. Initially, KFS conducted archival research and interviews to prepare a background history of the project area and related resources. KFS then conducted field investigations, photographing each resource, compiling descriptive field notes, and assessing integrity and condition. Evaluation of the National Register eligibility of each resource followed completion of the documentary and field investigations. This report presents the results and conclusions of the investigations and evaluation.

KFS determined that none of the five identified resources appears to meet National Register eligibility criteria. A sixth resource, the James Morrow House (N-224), was previously listed in the National Register of Historic Places (8/21/83) and did not warrant any further investigation.

• TABLE OF CONTENTS

Abstract

Table of Contents

List of Figures

I.	Introduction.....	1
II.	Methodology	3
III.	Cultural History.....	4
IV.	Research Design	8
V.	Analysis.....	13
VI.	Conclusion.....	17
VII.	Recommendations	18
VIII.	Bibliography.....	19

Figures

APPENDIX A: Cultural Resource Survey Forms

APPENDIX B: Vitae

• LIST OF FIGURES

- Figure 1. Area Context Map.
- Figure 2. Project Location Map.
- Figure 3. Resource Location Map.
- Figure 4. Henry Heald, *Roads of New Castle County, Delaware* (1820).
- Figure 5. S.M. Rea & J. Price, *Map of New Castle County, Delaware* (1849).
- Figure 6. Pomeroy & Beers, *Atlas of the State of Delaware* (1868).
- Figure 7. G.M. Hopkins, *Map of New Castle County, Delaware* (1881).
- Figure 8. Wm. G. Baist, *Atlas of New Castle County, Delaware* (1893).

I. INTRODUCTION

I. INTRODUCTION

This report documents the results and conclusions of a survey of historic architectural resources within a one-thousand-foot-wide study corridor that extends approximately one mile along State Route 273 (Ogletown Road) between Marrows Road (County Road 351) and the approximate vicinity of the Amtrak Railroad lines in White Clay Creek Hundred, New Castle County, Delaware (see Figures 1, 2, and 3). Kise Franks & Straw (KFS) completed the survey in the winter of 1993 in response to plans by the Federal Highway Administration, in conjunction with the Delaware Department of Transportation, for roadway widening and dualization.

Federal law, specifically Sections 106 and 110 of the National Historic Preservation Act of 1966 (as amended), mandates that all federal agencies undertaking projects that effect or have the potential to effect historic resources must undergo Section 106 Review. The Section 106 Review process requires that federal agencies take into account how their undertakings may affect historic resources and allow the Advisory Council on Historic Preservation (ACHP), an independent federal review agency chartered by Congress, the opportunity to comment on such undertakings. For the purposes of Section 106 Review, historic resources are identified as buildings, districts, structures, objects, and sites listed on or determined eligible for listing on the National Register of Historic Places, the nation's official list of historic resources deemed worthy of preservation.

The Section 106 Review process involves the identification of historic resources, evaluation of how the federal agency's undertaking will or will not affect the resources, and mitigation of the adverse or potentially adverse effects of the undertaking upon the resources. This three-step process involves the federal agency in consultations with the State Historic Preservation Office (SHPO) of the state in which the historic resources are located. The federal agency, in consultation with the SHPO, identifies historic resources that may be affected by the undertaking, evaluates the National Register eligibility of the identified resources, determines the effect of the undertaking upon resources eligible for the National Register, and develops mitigation measures for eligible resources adversely affected by the undertaking. In most cases, the period of review and consultation leads to the formulation of a Memorandum of Agreement (MOA) among the federal agency, the SHPO, the ACHP, and any other parties with an established interest in the matter. The MOA outlines each party's responsibilities for carrying out the intent of the agreement. Because the planned improvements to State Route 273 are being proposed by a federal agency, and because the undertaking has the potential to affect historic resources, the Section 106 Review process has been initiated.

The Delaware Department of Transportation hired the Historic Preservation Group of Kise Franks & Straw (KFS) to complete the historic resource identification and evaluation portions of the Section 106 Review process. KFS established the Area of Potential Effects (APE), "the geographic area within which [the] undertaking may cause changes in the character of or use of historic properties," based upon visual inspection of the project area.¹ KFS conducted background research to establish the history of the APE and aid in the identification of specific historic resources within the area. This research effort entailed a search of the SHPO's resource files, a review of pertinent primary and secondary source materials, and interviews with knowledgeable local residents and officials. Upon completion of the background research, KFS surveyed all resources located within the project area that appeared to be more than fifty years of age by means of field notes and photography. Five resources were selected for survey purposes because they appeared to

¹36 Code of Federal Regulations 800.2 (c).

meet the age criterion. A sixth resource, the James Morrow House, was previously listed on the National Register. Based upon the results of the background research and field investigations, KFS evaluated the potential National Register eligibility of each of the five resources.

KFS concluded that none of the five resources appears to meet National Register eligibility criteria. This report presents the results of the investigative and evaluative process for review by the Federal Highway Administration, the Delaware Department of Transportation, and the Delaware Historic Preservation Office.

As noted above, this study has been conducted in accordance with, and pursuant to, federal laws and regulations. The applicable statutes and directives include Section 101 (b)(4) of the National Environmental Policy Act of 1966, as amended; Sections 1(3) and 2(b) of Executive Order 11593, "Protection and Enhancement of Cultural Environment;" Sections 106 and 110 of the National Historic Preservation Act of 1966, as amended; 36 CFR Parts 60 and 63, "National Register of Historic Places;" and 36 CFR Part 800, "Procedures for the Protection of Historic Properties."

II. METHODOLOGY

II. METHODOLOGY

A windshield survey of the State Route 273 project area in the winter of 1993 revealed historic resources that appear to be fifty years of age or older and have the potential to be affected by the proposed road improvements.

Upon completion of the windshield survey, background research was initiated to gain an understanding of the area's historical and architectural development and to identify and assess previously prepared documentation, thus avoiding unnecessary duplication of efforts. Initially, KFS conducted a thorough search of pertinent architectural and historical literature to identify historical information on the resources identified during the windshield survey. Appropriate primary and secondary source materials pertaining to the project area were also consulted. KFS conducted this research at the Newark Public Library in Newark and the Delaware Historical Society in Wilmington. Interviews conducted with local property owners and knowledgeable residents provided considerable information on the development of the project area in the twentieth century.

KFS examined the holdings of the Delaware Historic Preservation Office in Dover to identify cultural resource reports and studies of the project area. Two archeological reports for sections of Route 273 east of the project area have been completed. Coleman, Hoseth, and Custer undertook a Phase II archeological investigation of the Ogletown Interchange in 1987 and Brown, Basalik, and Tabachnick performed archeological investigations along the stretch of Route 273 between Delaware Route 7 and US Route 13 in 1990. A hundred-wide multiple resource National Register nomination, which included the James Morrow House (N-224), was completed in 1982. As a result of being listed on the Register, the Morrow House did not require any further identification or evaluation. None of the other historic resources located within the project area were previously listed on the National Register of Historic Places.

Upon completion of the documentary research, KFS staff undertook field investigations designed to document the appearance, condition, and integrity of each of the five historic resources within the Area of Potential Effects (APE). Each resource was recorded photographically and field notes were taken describing the resource's appearance, integrity, and proximity to the proposed improvements. KFS also undertook extensive deed and property research at the New Castle County Recorder of Deeds in order to determine dates of construction and site-specific history for each of the five resources.

Using the data acquired during the background research and field survey phases, KFS evaluated each resource to determine whether or not it met National Register of Historic Places eligibility criteria, as outlined in 36 CFR 60.4. It was determined that none of the five identified resources appears to meet the criteria for listing on the National Register.

The final phase of the work entailed preparation of the survey report. All findings and analysis described above were documented in writing and assembled in standard report format. The following is a list of the resources surveyed by KFS in the State Route 273 project area.

- 705 Ogletown Road (N-12916)
- 709 Ogletown Road (N-12917)
- 1610-1612 Ogletown Road (N-12912)
- 1708 Ogletown Road (N-12913)
- 1712 Ogletown Road (N-12914)

III. CULTURAL HISTORY

III. CULTURAL HISTORY

The Dutch, represented by the Dutch West India Company, were the first to attempt a settlement in Delaware, near Lewes, in 1621. This short-lived venture was eclipsed by Swedish settlement attempts near Wilmington in the 1630s and 1640s. In 1655, Dutch governor Peter Stuyvesant attacked Swedish forts along the Delaware and gained control of the area. Stuyvesant made Fort Casimir, one of the captured Swedish forts, the capital of the Dutch colony and began planning an adjacent town, named New Amstel. Dutch claims ceased, however, in 1664, when the Duke of York sent an invasion fleet to Delaware and New York, exercising his claims to lands granted by Charles II. The result was the establishment of English rule in the Mid-Atlantic region. New Amstel was renamed New Castle by the English (Brown, et al. 1990: 20).

The early settlement pattern in New Castle County consisted of dispersed farmsteads, linked by former Indian trails and paths. These primitive paths, along with the Delaware River and its numerous creeks, were the principal means of transportation and communication between the settled areas. Farming was the chief occupation of the area's residents and the primary crop was wheat. Small ports were established at the navigable heads of the numerous creeks that flowed into the Delaware River. The creeks played an important dual role of carrying commercial traffic to and from inland ports and supplying water power for mills. Christiana, an important early trading center, was established at the navigable limit of the Christina River about 1660 (Brown, et al. 1990: 18). The first established segment of present Route 273 was between Christiana and New Castle.

In 1682, the granting of proprietary rights to William Penn and his representatives gave economic and political control of the Delaware region to Philadelphia, the new seat of government (Coleman, et al. 1987: 21). With the ascendancy of English rule and the establishment of Philadelphia, New Castle quickly lost its regional superiority as a trading center. The town did not possess an important mercantile or manufacturing base, and derived much of its prominence from being a regional governmental center (Brown, et al. 1990: 20). However, New Castle remained a major point of entry through the first quarter of the 1700s, mainly with Scotch-Irish immigrants (Brown, et al. 1990: 22).

By 1686 a bridge had been built over the Christina River and Christiana became a central intersection for the rudimentary roads present in New Castle County in the late seventeenth century. As a result of its accessibility by water and land, Christiana served as the eastern terminus for commerce shipped between the Chesapeake and Delaware Bays. The western terminus was Elkton, Maryland. The ultimate destinations for most goods were Philadelphia and Baltimore.

This trade corridor also established the small crossroads of Ogletown, northwest of Christiana, in the 1660s (Scharf 1888: 933). Ogletown was named after John Ogle, a prominent landowner whose holdings embraced the sites of both Ogletown and Christiana. During a brief period of its early history, the Ogletown vicinity was caught in the land grant confusion that existed between William Penn and Lord Baltimore. In 1684, George Talbot, a representative of Lord Baltimore, built a small fort at Ogletown to enforce Baltimore's land claims in the area (Conrad 1908: 496). A second account states that Talbot was dispatched to northern Delaware to establish settlements that would be loyal to Baltimore (Brown, et al. 1990: 18). Apparently, Talbot's overtures were not aggressive; by 1686, Penn's claims to the region were upheld and the issue was put to rest.

The earliest incarnation of the project segment of Route 273 was as a connector route between Ogletown and Newark. Newark was established in the early 1700s at the crossroads of two old Indian trails. As early as 1708, Scotch Presbyterians settled the area

west of Newark near the head of the Christina Creek. The establishment of a road between Ogletown and Newark linked Newark with Christiana and its commerce. This road was later extended further northwest to Nottingham, Pennsylvania, creating a new trade corridor that linked the agricultural fields of Chester County with the Christiana ports.

Throughout the eighteenth century, White Clay Creek Hundred remained an agricultural region. Development bypassed the town of New Castle in the 1700s and focused on Wilmington, which served as the region's commercial center. Existing roads were gradually improved and new roads were created to connect newly settled areas, mills, and emergent villages. Settlement in the hundred continued much as it had in the previous century and, as the population increased, marginal agricultural lands were settled. The influx of settlers to the area resulted in the formation of White Clay Creek Hundred in 1710.

By the start of the 1700s, the region was beginning to be recognized as a wheat and grain producing area (Coleman, et al. 1987: 24). Wheat represented a marketable good and allowed the settlers in the area to shift from a subsistence-oriented agricultural system to one which was market-oriented. Christiana's advantageous location was accentuated in the mid-eighteenth century as a grain transshipment center. Industry throughout the county was, for the most part, limited to mills used for agricultural processing. Within White Clay Creek Hundred, saw and grist mills were located along White Clay and Christina Creeks. There were no mills within the project area, which consisted solely of agricultural land in the eighteenth century.

King George II granted Newark corporate privileges in 1758. These privileges permitted the hamlet to host weekly market days and semi-annual fairs. The town grew modestly throughout the eighteenth century from the market trade as well as the nearby mills on White Clay Creek. In 1769, Newark Academy, the forerunner of the University of Delaware, was incorporated by John Penn. By the early nineteenth century, Newark was the largest town in White Clay Creek Hundred (see Figure 4).

During the Revolutionary War, the British blockaded the Delaware Bay, forcing trade to follow inland routes. Goods moving between Philadelphia and Baltimore were transported overland between Elkton and Christiana, and then on the Christina and Delaware Rivers. The British blockaded the Christina River for one month during their Philadelphia campaign (Brown et al. 1990: 23).

Within the project area, farmers most likely processed their wheat at Thomas Ogle's mill, a short distance down the Ogletown Road and sold their goods at the Newark farmer's market. Surplus goods may have been sent to Christiana for transshipment to Philadelphia and Wilmington. Throughout the late eighteenth and early nineteenth century, the road from Newark to Christiana was constantly being petitioned for straightening and other improvements (Coleman, et al. 1987: 30).

The region's general location between the major markets of Philadelphia and Baltimore was emphasized during the nineteenth century, when improved transportation links were established between the two cities. Canals, turnpikes, and railroads transformed the landscape and increased communication and commerce, both locally and regionally. The result was a slight increase in commercial and residential development in and around New Castle and Wilmington. The majority of the county, including the project area, remained rural farmland.

In the vicinity of the project area, the New Castle Turnpike Company was chartered by the Delaware General Assembly on January 30, 1811. Work on the turnpike commenced

August 1, 1811 in New Castle and was completed to Clark's Corner on January 8, 1813 (Scharf 1888: 417). The New Castle & White Clay Hundred Turnpike was incorporated on February 1, 1813. The pike was proposed to extend from the New Castle Turnpike at Clark's Corner to the Maryland line, via Christiana and Newark. Commissioners were named but the road was never built. Little is known of the proposed route of this turnpike, but most likely it would have incorporated the project area between Ogletown and Newark. The New Castle Turnpike was eventually extended from Clark's Corner through Christiana to Frenchtown, near Elkton, in 1817. In January 1816, an act of incorporation was passed providing for a turnpike connecting Stanton with Maryland, via Newark (Scharf 1888: 418). The turnpike crossed White Clay Creek north of the project area and intersected with the project area just east of Newark. This road was straightened in the twentieth century for more direct access to Newark and now forms part of Route 2 (Capitol Trail).

In 1822, new plans were made for a canal that would bypass both Christiana and Elkton, isolating New Castle and the Christina River towns from Chesapeake commerce. The Chesapeake & Delaware Canal, financed largely by Philadelphia business interests, was completed in 1829 and initiated the economic decline of Christiana, Ogletown, Newport, and Stanton. Commerce previously shipped overland via Christiana was now sent through the canal. Residents of New Castle, in an attempt to compete with the canal, commenced plans to build a railroad (Brown, et al. 1990: 27). The New Castle & Frenchtown Railroad, completed in 1831, was the first railroad in Delaware. This line was soon after eclipsed by the Philadelphia, Wilmington & Baltimore Railroad, established in 1838. This railroad served to both strengthen towns located along its line, such as Newark, and contribute to the decline of the river towns initiated by the C & D Canal.

In spite of the transportation improvements that occurred throughout the region, the project area experienced very little change in the nineteenth century. The area was characterized by large-acre agricultural parcels and the chief occupation was farming. The 1849 Rea & Price atlas reveals the project area contained two dwellings but only one landowner, George Janvier (see Figure 5). Janvier's holdings formed the primary parcel within the project area. Names associated with this parcel after Janvier included Laws, Morrow, Richards, and Stafford. The Philadelphia, Wilmington & Baltimore Railroad intersected Ogletown Road south of the project area, however there were no railroad-related facilities constructed at Ogletown Road.

The 1868 Pomeroy & Beers atlas reveals the rural nature of Ogletown Road and the surrounding countryside in the third quarter of the nineteenth century (see Figure 6). The project area was composed of large-acre agricultural estates, including Hermitage, Maple Springs, and Fairview [present-day James Morrow House (N-224) listed on the National Register 8/21/83]. Ogletown consisted of a crossroads community with a hotel and schoolhouse. Newark was the largest town in the hundred and contained rail facilities and industrial factories located on White Clay Creek. The 1881 Hopkins and 1893 Baist atlases (see Figures 7 & 8) essentially reveal the same information. By 1881, Homewood Driving Park, a half-mile trotting track, had been built on the edge of the project area. The Baltimore & Ohio Railroad was established in 1886 and the 1893 atlas reveals the B & O's Lumbrook Station north of the project area.

The project area began a gradual transformation from farmland to suburb in the second decade of the twentieth century when the Morrow family sold off a three-acre tract and 709 Ogletown Road (N-12917) was built. In the 1920s, automobile usage increased and residential development occurred on former agricultural parcels around Newark. During this era, four suburban residential resources were constructed in the project area: 705 Ogletown Road (N-12916), 1610-1612 Ogletown Road (N-12912), 1708 Ogletown Road (N-12913), and 1712 Ogletown Road (N-12914). The Homewood Driving Park was

removed in the mid-twentieth century to accommodate the construction of a shopping center on the west side of Marrows Road.

Within the last fifty years, corporate, commercial and light industrial facilities have been built within the project area, transforming the former agricultural landscape into a disjointed roadscape. The project area is presently comprised of residences, factories (e.g. Motor Wheel Corporation), an auto dealership, corporate offices, and small-scale commercial businesses. The sole agricultural parcel in the project area is the James Morrow House (N-224).

IV. RESEARCH DESIGN

IV. RESEARCH DESIGN

The goal of this study was to evaluate Ogletown Road (Route 273) to determine if, within the area of potential effect, any historic resources were eligible for the National Register. This was accomplished by determining relevant historic contexts, consistent with National Register Bulletin 15 and the *Delaware Comprehensive Historic Preservation Plan*, and establishing evaluation criteria for the historic resources. The primary components of a historic context include a historic theme, a geographic zone, and a chronological period. A windshield survey performed for proposal purposes revealed the historic resources in the project area consisted of agricultural and suburban residential resources that appeared to date from the late nineteenth century or the first half of the twentieth century. Thus, the historic contexts for the resources in the project area were defined as Agriculture in the Piedmont Zone, 1880-1940 +/- and Settlement Patterns and Demographic Change in the Piedmont Zone, 1880-1940 +/-.

The *Delaware Comprehensive Historic Preservation Plan* identifies historic contexts that are considered priority contexts for above-ground resources. Resources that pertain to these contexts "reflect the most vulnerable resources constructed in the most formative time periods and located in the most threatened geographic areas."¹ The sole priority identified for the Piedmont Zone, the location zone for the project area, is the theme Settlement Patterns and Demographic Change for the time periods 1770-1830 and 1830-1880. Therefore, neither of the contexts identified for the project area is acknowledged as a historic context priority.

Agriculture in the Piedmont Zone, 1880-1940 +/-

The agricultural past of the project area is evident from an examination of late-nineteenth century atlases, which depict large-acre estates in the project area. The Preservation Plan notes the Piedmont zone is presently undergoing intensive development, most of which is "occurring in agricultural areas that have lost their integrity."² Based upon the windshield survey and examination of a USGS quad map (Newark East), the project area appears to fit within this category. The USGS map reveals large commercial and/or industrial buildings, including an industrial park, have been built within the project area since 1970. Likewise, large-scale suburban residential developments have also been constructed in the general vicinity of the project area. Thus, informational needs required from the field survey pertained to the incidence and integrity of the project area's agricultural properties.

The agricultural context can include a number of different property types, ranging from farmhouses to barns and the array of auxiliary outbuildings. The *Historic Context Master Reference and Summary*, the companion to the Preservation Plan, identifies example property types for Agriculture in the Piedmont Zone, 1880-1940 as bank barns, truck and dairy farms, and rural estates. As noted above, historic atlases depicted large-acre estates within the project area. Thus, expected property types, if agricultural properties were located, encompassed everything from farmhouses to minor outbuildings.

¹David L. Ames, Mary Helen Callahan, Bernard L. Herman and Rebecca J. Siders, *Delaware Comprehensive Historic Preservation Plan* (Newark, DE: Center for Historic Architecture and Engineering, 1989), 83.

²*Ibid.*, 82.

Settlement Patterns and Demographic Change in the Piedmont Zone, 1880-1940 +/-

The *Delaware Comprehensive Historic Preservation Plan* defines the historic theme Settlement Patterns and Demographic Change as "the historical succession of settlement patterns and development accommodated by, and reflected in, the landscape."³ For the Urbanization and Early Suburbanization (1880-1940 +/-) time period, the Preservation Plan identified example property types associated with the theme, such as powder and chemical factories, planned residential communities, bungalows, trolley lines, gas stations, and resort hotels.

Property types classified within this context are largely related to the suburbanization of the zone's urban areas, Wilmington and Newark. The theme of residential suburbanization with regard to the city of Wilmington has been examined.⁴ The authors of that study noted the primary property types of residential suburbanization are the self-contained subdivision, defined as a community of specifically planned streets and building lots, and the individual suburban dwelling. However, "the primary criterion for determining a dwelling's eligibility for inclusion in the historic context for suburbanization is that the building was constructed in an area that meets the definition of a subdivision."⁵ Therefore, individual suburban dwellings were excluded from the Wilmington context unless they were a component of a larger subdivision. As a result, informational needs required from the field survey pertained to the relationship and association between the various suburban dwellings in the project area. The Wilmington context provides a discussion of the physical characteristics of suburban dwelling styles as well as criteria of integrity and significance specific to dwellings.⁶

In conjunction with the submittal of the draft report, consultation with the Delaware SHPO identified a third property type of residential suburbanization not addressed in the Wilmington study -- the roadside subdivision. The following is a discussion of this property type.

Residential Roadside Suburbanization, 1910-1945 +/-

(Note: The *Delaware Comprehensive Historic Preservation Plan* defines the Urbanization and Early Suburbanization chronological period as 1880-1940 +/- . However, since the roadside subdivision relates to the emergence of the automobile, the chronological period is more appropriately defined as 1910-1945 +/- .)

The establishment of residential roadside subdivisions in the early and mid-twentieth century was directly related to the emergence of the automobile. The automobile made accessible land bordering corridors, such as state and county highways, that ran between population nodes or centers. This land remained largely undeveloped during the suburban boom of the late nineteenth century, which relied upon rail transportation such as trolleys and trains. The suburbs that grew up around the suburban rail stations did not extend far beyond city, town, or village limits. The land bordering the automobile corridors, more often than not, originally consisted of large-acre agricultural parcels and estates. These large-acre parcels became prime real estate with the increase in automobile usage and were

³Ibid., 28.

⁴Susan Mulchahey Chase, David L. Ames, and Rebecca J. Siders, *Suburbanization in the Vicinity of Wilmington, 1880-1950 +/-: A Historic Context* (Newark, DE: Center for Historic Architecture and Engineering, 1992).

⁵Ibid., 62.

⁶Ibid., 62-64.

heavily pursued by developers, who usually transformed the landscape into planned subdivisions.

One form of the residential roadside subdivision property type can be traced to the automobile-facilitated, twentieth-century suburban boom. In this form, only a property's frontage along a road, instead of a whole parcel, was made available for development. This arrangement provided a farmer or estate owner with quick capital and still left a majority of the parcel as open land. In such a land division, no attempt is made to create a community like those envisioned in self-contained subdivisions. For the purposes of this report, this property type will be referred to as the *speculative roadside subdivision*. A second form of the residential roadside subdivision property type results from the process whereby a large agricultural parcel or estate is subdivided to accommodate successive generations. This property type will be referred to as the *agricultural roadside subdivision*.

Although the division of agricultural lands to accommodate heirs was traditional practice in rural areas for centuries, the property type identified in this report differs from earlier such land divisions in a few important aspects, besides architecture. The twentieth century agricultural subdivision was characterized by small, residential parcels that had a stronger physical relationship with the road than with the farm or estate. Their eighteenth and nineteenth century counterparts consisted of larger-acre parcels, often with dwellings set away from the road. In general, these earlier subdivisions provided an heir with enough land to live upon, if so required.

The differences between the speculative and agricultural roadside subdivisions were primarily physical and were the result of the separate objectives of the original land division. The goal of the speculative subdivision was to generate capital, regardless if the developer was a farmer or speculator. To these ends, the speculative subdivision generally limited individual property acreage to allow more dwellings and therefore maximize profit. The result was often a repetitious strip with little physical correlation to the larger parcel that generated the subdivision. In turn, the purpose of the agricultural subdivision was to house descendants and, as a result, the landscape wasn't as aggressively developed as the speculative subdivision. In some cases, the parcels in an agricultural subdivision were non-contiguous. In general, the agricultural subdivision maintained a greater physical correlation with its larger parcel because more of the original rural landscape remained intact.

Speculative roadside subdivisions share common physical characteristics that aid in their identification. As noted above, this sub-type consists of a roadside strip that isn't part of a larger development. For the purposes of this report, a strip is defined as at least two adjacent parcels that are only one parcel deep off the road. The dwellings were usually built at the same time, and generally exhibit a uniformity in size, height, massing, and architectural style. Within the strip, the parcels should be a similar size or, if not a similar size, a similar property width. Dwellings built within the parcel should be located near the road and share a similar setback. The location of the dwellings and the width of the property combine to give the subdivision a repetitive pattern. If the dwellings are set too far off the road, or if adjacent properties have variable widths, then the feeling of a strip isn't present and the subdivision would be considered to have less integrity than if these characteristics were present.

Agricultural roadside subdivisions also share common characteristics that aid in their identification. Of primary importance is the presence of the original farm or estate parcel, even moreso if the farmhouse or estate house are extant. The roadside dwellings tend to be built a different times in different styles, related to the generational changes within the family. Unlike the speculative subdivision, lot size or width need not be uniform, since a

repetitive pattern is not a physical characteristic to this property type. The physical relationship between the original parcel and the roadside parcels is the major determinant of the integrity of this sub-type.

To determine which sub-type is applicable to a subdivision whose origins are unclear, the property history of the parcels should be researched. The subdivision will be documented in deed records and possibly, development plans, if submitted.

Criteria for Evaluation

Once a property's historic context has been identified, it is evaluated for the National Register of Historic Places. The criteria for evaluating a property's eligibility for listing in the National Register (36 CFR 60.4, "Criteria for Evaluation") are as follows:

The quality of significance in American history, architecture, archeology, engineering and culture is present in districts, sites, buildings, structures, and objects that possess integrity of location, design, setting, materials, workmanship, feeling, and association, and

(a) that are associated with events that have made a significant contribution to the broad patterns of our history; or

(b) that are associated with the lives of persons significant in our past; or

(c) that embody the distinctive characteristics of a type, period, or method of construction, or that represent the work of a master, or possess high artistic values, or that represent a significant and distinguishable entity whose components may lack individual distinction; or

(d) that have yielded, or may be likely to yield information important in pre-history or history.

To be listed in the National Register of Historic Places, "a property must not only be shown to be significant under the National Register criteria, but it also must have integrity."⁷

Agriculture in the Piedmont Zone, 1880-1940 +/-

Properties eligible for inclusion in the agricultural context could be nominated under Criteria A, B, or C. Criterion D is most commonly applied to archeological sites. Of the five types of historic property defined by the National Register, the two most commonly associated with the agricultural context are districts (e.g. estates or complexes) and buildings (e.g. farmhouses, barns, corncribs, granaries).

Integrity issues that are important within this context include issues related to an individual resource's design and materials, such as whether or not a resource has experienced alteration or deterioration. Also important is location, such as whether or not a resource has been moved, and setting with regard to associated agricultural fields and outbuildings.

⁷National Park Service, *National Register Bulletin 15: How to Apply the National Register Criteria for Evaluation* (1990; revised 1991), 44.

Settlement Patterns and Demographic Change in the Piedmont Zone, 1880-1940 +/-

Suburban residential properties eligible for inclusion in the Settlement Patterns and Demographic Change context could be nominated under Criteria A, B, or C. Criterion D is most commonly applied to archeological sites. Of the five types of historic property defined by the National Register, the two most commonly associated with this context are districts (e.g. subdivisions) and buildings (e.g. individual suburban dwellings). A key significance issue for an individual suburban dwelling is whether or not there are adjacent suburban dwellings. If not, it is likely that such a resource loses significance as a suburban dwelling, however, there may be other aspects that could make it eligible for the National Register under Criteria A, B, or C.

Integrity issues that are important within this context include issues related to an individual resource's design and materials, such as whether or not a resource has experienced alteration or deterioration. Integrity of design is critical with regard to the design of a subdivision. Also important is location, such as whether or not a resource has been moved, and setting, which is particularly relevant with regard to subdivisions.

Residential Roadside Suburbanization, 1910-1945 +/-

Properties eligible for inclusion in the roadside residential subdivision context could likewise be nominated under Criteria A, B, or C. Criterion D is most commonly applied to archeological sites. Of the five types of historic property defined by the National Register, two are associated with the residential roadside suburbanization context: districts (e.g. subdivisions) and buildings (e.g. individual suburban dwellings). However, individual suburban dwellings would ordinarily not be significant unless as part of a larger roadside subdivision. Therefore, a large majority of the nominations related to the residential roadside suburbanization context will be as districts.

The more important integrity elements with relation to this context are those that convey the relationship between resources, such as are design, setting, and feeling.

V. ANALYSIS

V. ANALYSIS

Field survey and research revealed the project area contained five historic resources: four suburban dwellings (705, 1610-1612, 1708, and 1712 Ogletown Road) and a former farmhouse that is presently used for commercial purposes (709 Ogletown Road). The higher incidence of suburban dwellings, moreso than agricultural-related properties, is most likely due to the project area's close proximity to Newark. The only property in the project area currently used for agricultural purposes is the James Morrow House (N-224), previously listed in the National Register of Historic Places (8/21/83).

The relationship and association among the suburban dwellings was more fully explored during the field survey. Three dwellings (1610-1612, 1708, and 1712 Ogletown Road) initially appeared to relate to the agricultural roadside subdivision sub-type of the residential roadside suburbanization property type. However, research revealed that two of the resources (1610-1612 and 1708 Ogletown Road) were built to house tenants as opposed to family.

1) 705 Ogletown Road (N-12916)

Property type: Suburban Dwelling

Historic Context: Settlement Patterns and Demographic Change in the Piedmont Zone, 1880-1940 +/-

New Castle County property records indicate that Samuel E. Dameron purchased this parcel from Julia Huber in October 1924. Dameron subdivided the property and this resource was built circa 1925. The resource is a two-and-one-half story, two-bay, brick Foursquare-style dwelling and is not associated with a larger suburban subdivision. Features of the Foursquare style exhibited at 705 Ogletown Road include the box form, the low hipped roof with exposed rafters, the wraparound porch, and the three-over-one windows. In 1963, this parcel was consolidated with the adjacent property at 709 Ogletown Road (N-12917) to form one property.

705 Ogletown Road retains its integrity, but does not appear eligible for listing on the National Register of Historic Places under Criteria A, B, C, or D. Visual inspection does not indicate that the resource is significant for its method of construction or architecture (Criterion C). 705 Ogletown Road is a typical rather than innovative or distinguished example of the Foursquare style. Moreover, archival research, interviews with knowledgeable local residents, and investigation of cultural resource studies at the Delaware SHPO have produced no information indicating the resource to be significant for its associations with significant events and persons, or for its ability to yield information important in prehistory or history (Criteria A, B, and D).

2) 709 Ogletown Road (N-12917)

Property type: Farmhouse

Historic Context: Agriculture in the Piedmont Zone, 1880-1940 +/-

709 Ogletown Road was constructed circa 1910 as a farmhouse. The three-acre property was originally part of the large holdings of the Morrow family, proprietors of the James Morrow House (N-224), from 1875 to 1909. In 1909, the property was sold to William J. Lovett of Philadelphia for \$1075. The deed for this transaction revealed the property contained a frame dwelling. Thirteen months later, Lovett sold the same tract to Charles R. Thomas for more than twice his purchase price (\$2200). Interior inspection of the dwelling disclosed an earlier stone foundation under the existing rusticated concrete block foundation. Due to the increase in price and early-twentieth-century architectural details of the dwelling, such as the foundation, standard-width wood flooring, and front and side

porch details, it appears Lovett most likely removed the former frame dwelling and erected the present dwelling atop the former foundation. The 1920 census listed Thomas as a farmer of his "own accounts." The resource is a two-and-one-half story, two-bay, vernacular frame dwelling.

709 Ogletown Road does not appear eligible for listing on the National Register of Historic Places under Criteria A, B, C, or D. Visual inspection does not indicate that the resource is significant for its method of construction or architecture (Criterion C). Post-1950 alterations, including the application of stucco siding and aluminum window trim, have severely compromised the original design and materials of the resource. Also, the resource's original agricultural setting is no longer extant and has been replaced by a light industrial commercial establishment. Moreover, archival research, interviews with knowledgeable local residents, and investigation of cultural resource studies at the Delaware SHPO have produced no information indicating the resource to be significant for its associations with significant events and persons, or for its ability to yield information important in prehistory or history (Criteria A, B, and D).

3) 1610-1612 Ogletown Road (N-12912)

Property type: Suburban Dwelling

Historic Context: Settlement Patterns and Demographic Change in the Piedmont Zone, 1880-1940 +/-

(As 1610-1612 Ogletown Road was not built to house a family member, it is precluded as a form of the agricultural roadside subdivision sub-type of the residential roadside suburbanization property type, as currently defined.)

This property was originally part of a larger 300-acre tract of land purchased by John F. Richards in 1911. After Richards's acquisition, he set aside a five-acre tenant farm comprised of an circa 1756 farmhouse, a brick barn, a stone barn, and a chicken coop. The property comprising 1610-1612 Ogletown Road was once part of this tenant farm. A circa 1925 fire destroyed the 1756 farmhouse and the only extant building related to the tenant farm is the chicken coop, located on this property. According to Richards' grandson, Robert Stafford, 1610-1612 Ogletown Road and its associated garage were constructed circa 1936. Bricks were salvaged from the former brick tenant barn, demolished in 1936, for the construction of 1610-1612 Ogletown Road. After Richards' death in 1947, his granddaughter and her husband, Emilie and William Wilhelm, acquired the property. 1610-1612 Ogletown Road is a two-and-one-half-story, Foursquare-style brick duplex dwelling and is not associated with a larger suburban subdivision. Features of the Foursquare style exhibited at 1610-1612 Ogletown Road include the box form and the full-width front porch.

1610-1612 Ogletown Road retains its integrity, but does not appear eligible for listing on the National Register of Historic Places under Criteria A, B, C, or D. Visual inspection does not indicate that the resource is significant for its method of construction or architecture (Criterion C). 1610-1612 Ogletown Road is a typical rather than innovative or distinguished example of a Foursquare-style dwelling. Moreover, archival research, interviews with knowledgeable local residents, and investigation of cultural resource studies at the Delaware SHPO have produced no information indicating the resource to be significant for its associations with significant events and persons, or for its ability to yield information important in prehistory or history (Criteria A, B, and D).

4) 1708 Ogletown Road (N-12913)

Property type: Suburban Dwelling

Historic Context: Settlement Patterns and Demographic Change in the Piedmont Zone, 1880-1940 +/-

(As 1708 Ogletown Road was not built to house a family member, it is precluded as a form of the agricultural roadside subdivision sub-type of the residential roadside suburbanization property type, as currently defined.)

This property was originally part of a larger 300-acre tract of land purchased by John F. Richards in 1911. After Richards's acquisition, he set aside a five-acre tenant farm comprised of an circa 1756 farmhouse, a brick barn, a stone barn, and a chicken coop. The property comprising 1708 Ogletown Road was once part of this tenant farm. A circa 1925 fire destroyed the 1756 farmhouse and this dwelling was constructed circa 1926 on the former location of the eighteenth-century farmhouse. After Richards' death in 1947, the property was sold to his daughter and her husband, Mildred and Courtland Nesbitt. The Nesbitt's continued to rent the property until it was sold out of the family in 1979. 1708 Ogletown Road is a two-and-one-half story, three-bay, frame Foursquare-style dwelling and is not associated with a larger suburban subdivision. Features of the Foursquare style exhibited at 1708 Ogletown Road include the box form and the low hipped roof with wide overhanging eaves.

1708 Ogletown Road does not appear eligible for listing on the National Register of Historic Places under Criteria A, B, C, or D. Visual inspection does not indicate that the resource is significant for its method of construction or architecture (Criterion C). Post-1950 alterations, including the application of exterior vertical board siding and the removal of original porches, have severely compromised the original design and materials of the resource. Moreover, archival research, interviews with knowledgeable local residents, and investigation of cultural resource studies at the Delaware SHPO have produced no information indicating the resource to be significant for its associations with significant events and persons, or for its ability to yield information important in prehistory or history (Criteria A, B, and D).

5) 1712 Ogletown Road (N-12914)

Property type: Suburban Dwelling/ Agriculture Roadside Subdivision

Historic Context: Residential Roadside Suburbanization in the Piedmont Zone, 1880-1940 +/-

John F. Richards purchased this property from the Morrow family in 1911 as part of a 300 acre farm. In 1941, Richards's sold this tract of land to his daughter and her husband, Mildred and Courtland Nesbitt. According to Richards's grandson, Robert Stafford, this dwelling was built circa 1942 by the Nesbitts. The resource is a one-and-one-half story, two-bay, brick Cape Cod Colonial-style dwelling. Features of the Cape Cod Colonial style exhibited at 1712 Ogletown Road include the overall symmetrical form and fenestration, the side gabled roof with front dormers, and the central entranceway with a classical surround and decorative door hood.

1712 Ogletown Road does not appear eligible for listing on the National Register of Historic Places under Criteria A, B, C, or D. Visual inspection does not indicate that the resource is significant for its method of construction or architecture (Criterion C). Post-1950 alterations, including window replacement and the addition of a large pole barn on the rear of the dwelling, have severely compromised the original design and materials of the resource. Moreover, archival research, interviews with knowledgeable local residents, and investigation of cultural resource studies at the Delaware SHPO have produced no

information indicating the resource to be significant for its associations with significant events and persons, or for its ability to yield information important in prehistory or history (Criteria A, B, and D).

VI. CONCLUSION

VI. CONCLUSION

Five individual resources were surveyed in the field. None of the resources were previously listed in or determined eligible for the National Register. This survey concludes that none of the resources appear eligible, individually or as part of a district, for the National Register. A sixth resource, the James Morrow House (N-224), was previously listed in the National Register of Historic Places (8/21/83) and did not warrant any further investigation. Determination of effects for the project area will be limited to the James Morrow House.

VII. RECOMMENDATIONS

VII. RECOMMENDATIONS

No unanticipated contexts or property types were discovered in the project area. Background material, including cultural resource survey forms and photographs, are located at the Delaware State Historic Preservation Office, Dover, DE.

The following are historic context subjects that require further research and inspection:

The Wilmington suburbanization context excludes "isolated dwellings that were built in a geographic location that was distant from the city but which were not part of a specifically planned set of streets and building lots." The premise is that a suburban dwelling isn't significant as a form of suburbanization unless it is part of a larger subdivision. Nevertheless, individual resources still belong within a general suburbanization context and the motives and reasons behind their development require inspection. Therefore, a next step in the development of suburbanization contexts should focus on individual dwellings built outside the context of a larger subdivision.

A second area that requires further inspection is the role of tenant dwellings with regard to the agricultural roadside subdivision sub-type of the residential roadside suburbanization property type. 1610-1612 Ogletown Road and 1708 Ogletown Road are examples of resources built by an agricultural landowner, not to house future generations, but for rental purposes. Inspection should focus on whether the tenant dwelling property type has its own physical and associative characteristics with regard to roadside suburbanization of the first half of the twentieth century or whether it is essentially similar to the agricultural roadside subdivision sub-type.

VIII. BIBLIOGRAPHY

VIII. BIBLIOGRAPHY

- Ames, David L., Mary Helen Callahan, Bernard L. Herman and Rebecca J. Siders
1989 *Delaware Comprehensive Historic Preservation Plan*. Center for Historic Architecture and Engineering, University of Delaware, Newark, DE.
- Baist, W.G.
1893 *Atlas of New Castle County, Delaware*. W.G. Baist, Philadelphia.
- Beers, D.G.
1868 *Atlas of the State of Delaware*. Pomeroy and Beers, Philadelphia.
- Bennett, George Fletcher
1932 *Early Architecture of Delaware*. Bonanza Books, New York.
- Brown, Ann R., Kenneth J. Basalik, and Alan Tabachnick
1990 *Cultural Resources Assessment of Route 273, DE Route 7 to U.S. Route 13, New Castle County, Delaware*. Delaware Department of Transportation Archeological Series No. 76, Dover, DE.
- Chase, Susan Mulchahey, David L. Ames, and Rebecca J. Siders
1992 *Suburbanization in the Vicinity of Wilmington, 1880-1950 +/-: A Historic Context*. Center for Historic Architecture and Engineering, University of Delaware, Newark, DE.
- Coleman, Ellis C., Angela Hoseth, and Jay F. Custer
1987 *Phase I and II Archaeological Investigations of the Ogletown Interchange Improvements Project Area, Newark, Delaware*. Delaware Department of Transportation Archeological Series No. 76, Dover, DE.
- Conrad, Henry C.
1908 *History of the State of Delaware*. Volume II. Henry C. Conrad, Wilmington, DE.
- Cooch, Francis A.
1936 *Little Known History of Newark, Delaware and Its Environs*. The Press of Kells, Newark, DE.
- Del Sordo, Stephen, editor
1984 *To Build in the Best Manner: Vernacular Architecture in Middle Delaware*. Delaware Division of Historic and Cultural Affairs, Dover, DE.
- Delaware State Highway Department
1927 *Plan for Construction of Contract No. CN27*. On file at the Delaware Department of Transportation, Dover, DE.
- Delaware State Highway Department
1937 *Plan for Construction of Contract No. 525*. On file at the Delaware Department of Transportation, Dover, DE.
- Delaware State Highway Department
1958 *Plan for Construction of Contract No. 1378*. On file at the Delaware Department of Transportation, Dover, DE.

- Fitting, Gretchen, Richard Jett, and Valerie Cesna
1983 *White Clay Creek Hundred Multiple Resource Area National Register of Historic Places Inventory -- Nomination Form.* On file at the Delaware State Historic Preservation Office, Dover, DE.
- Heald, Henry
1820 *Roads of New Castle County.*
- Herman, Bernard L.
1987 *Architecture and Rural Life in Central Delaware, 1700-1900.* University of Tennessee, Knoxville.
- Herman, Bernard L., Rebecca J. Siders, David L. Ames, Mary Helen Callahan
1989 *Historic Context Master Reference and Summary.* Center for Historic Architecture and Engineering, University of Delaware, Newark, DE.
- Hopkins, G.M. & Co.
1881 *Map of New Castle County, Delaware.* G.M. Hopkins & Co., Philadelphia.
- Jackson, Kenneth T.
1985 *Crabgrass Frontier; The Suburbanization of the United States.* Oxford University Press, New York.
- Lancaster County Planning Commission
n.d. "Handbook For Livable Communities, Including Model Ordinances." Rough Draft.
- Lanier, Gabrielle M. and Bernard L. Herman
1992 *A Field Guide to Delaware Architecture.* Center for Historic Architecture and Engineering, University of Delaware, Newark, DE.
- League of Women Voters
1954 *Know Your Newark, Delaware.* The League of Women Voters, Newark, DE.
- Rea, S.M., and J. Price
1849 *Map of New Castle, Delaware.* Smith and Wister, Philadelphia.
- Scharf, J. Thomas
1888 *History of Delaware, 1609-1888.* Volumes I and II. L. J. Richards and Company, Philadelphia.
- Siders, Rebecca J., Bernard L. Herman, and David L. Ames
1991 *Agricultural Tenancy in Central Delaware, 1770-1900 +/-; A Historic Context.* Center for Historic Architecture and Engineering, University of Delaware, Newark, DE.
- Stafford, Anna M.
3/18/93 Personal interview with landowner along Ogletown Road.
- Stafford, Robert
5/7/93 Telephone interview with landowner along Ogletown Road.

Stilgoe, John R.

1988 *Borderland; Origins of the American Suburb, 1820-1939.* Yale University Press, New Haven.

Yaro, Robert D., Randall G. Arendt, Harry L. Dodson, and Elizabeth A. Brabec

1989 *Dealing With Change in the Connecticut River Valley: A Design Manual for Conservation and Development..* Lincoln Institute of Land Policy and the Environmental Law Foundation, Amherst, MA.

FIGURES

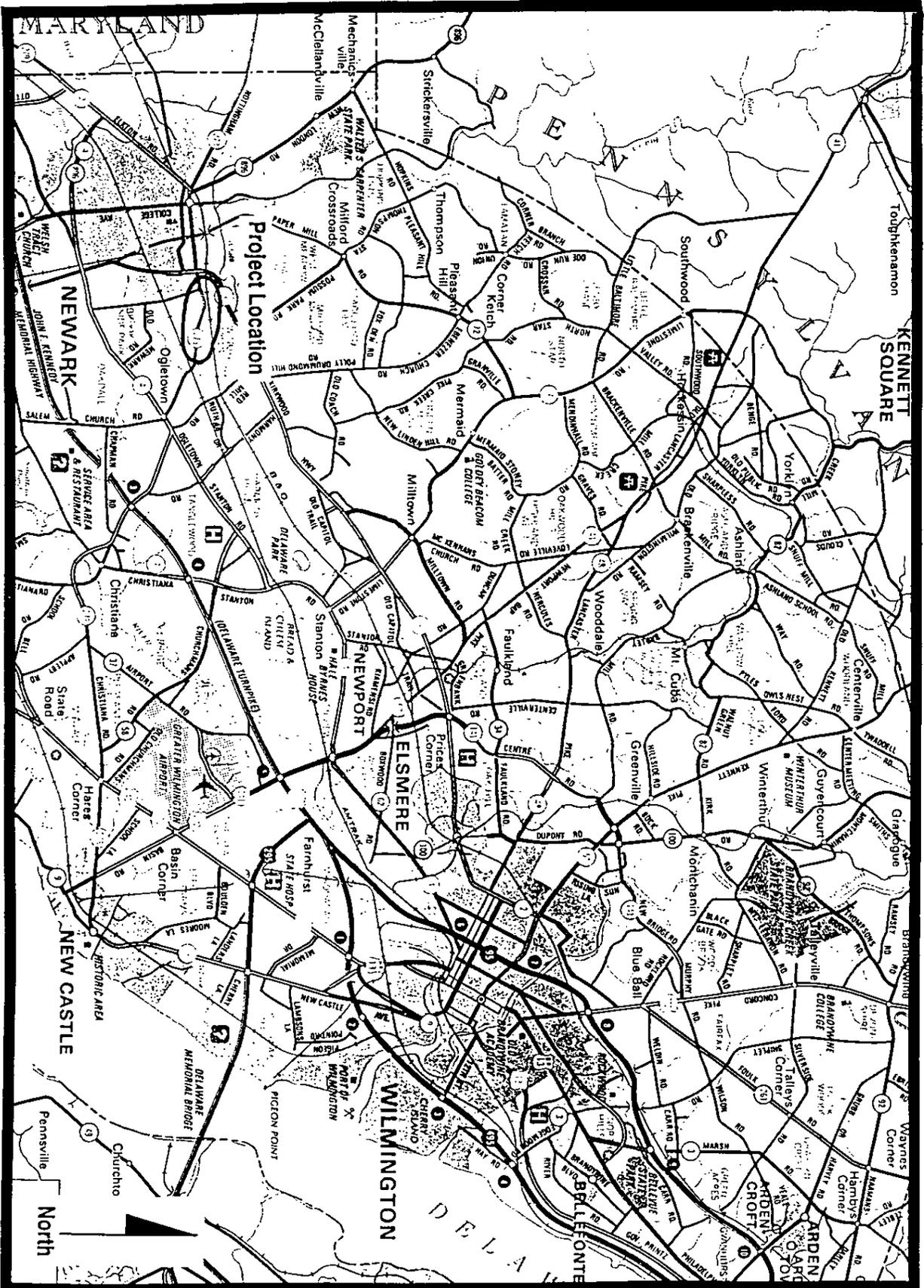


Figure 1. Area Context Map.
Scale: 1 inch = 2 miles

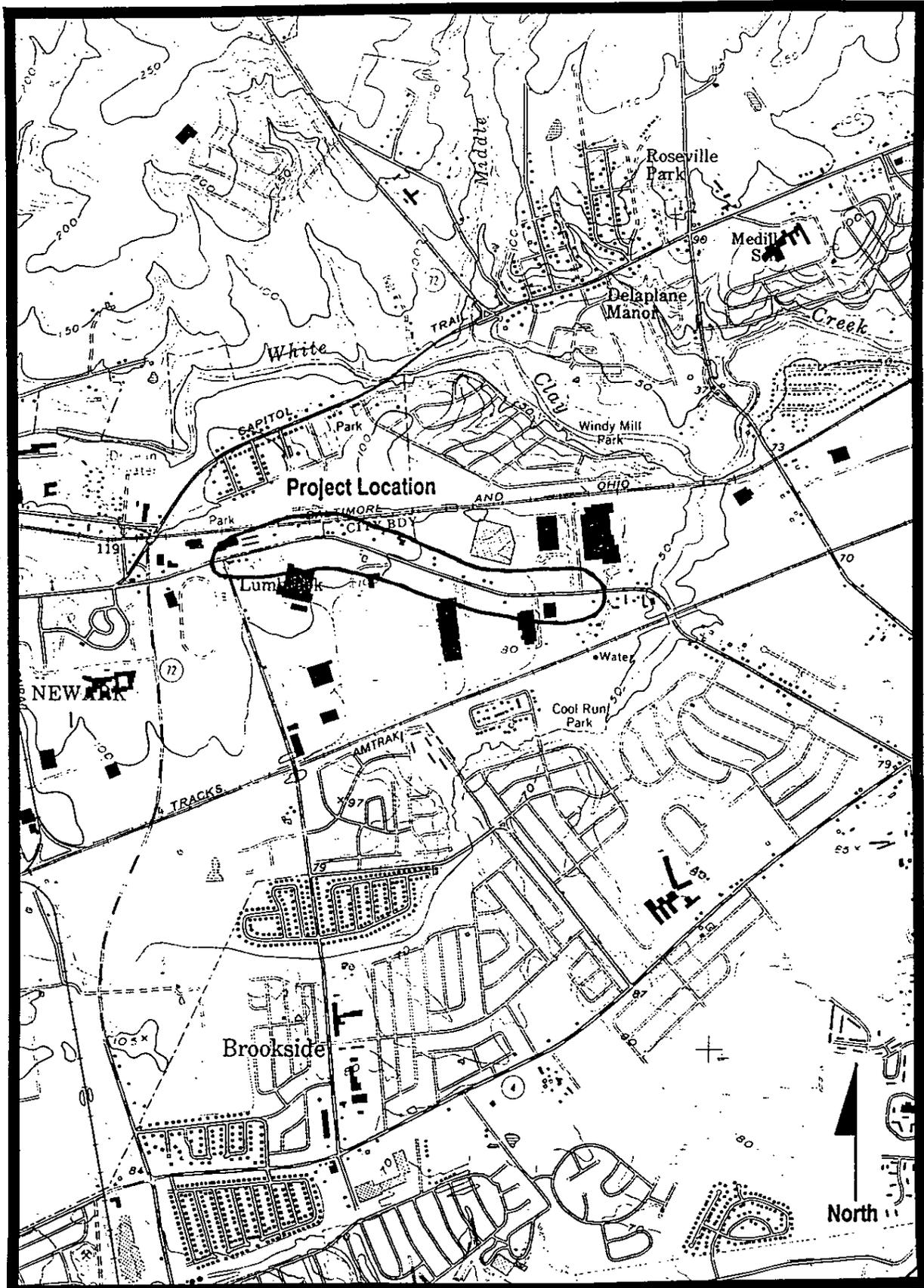


Figure 2. Project Location Map.
Scale: 1 inch = 2000 feet

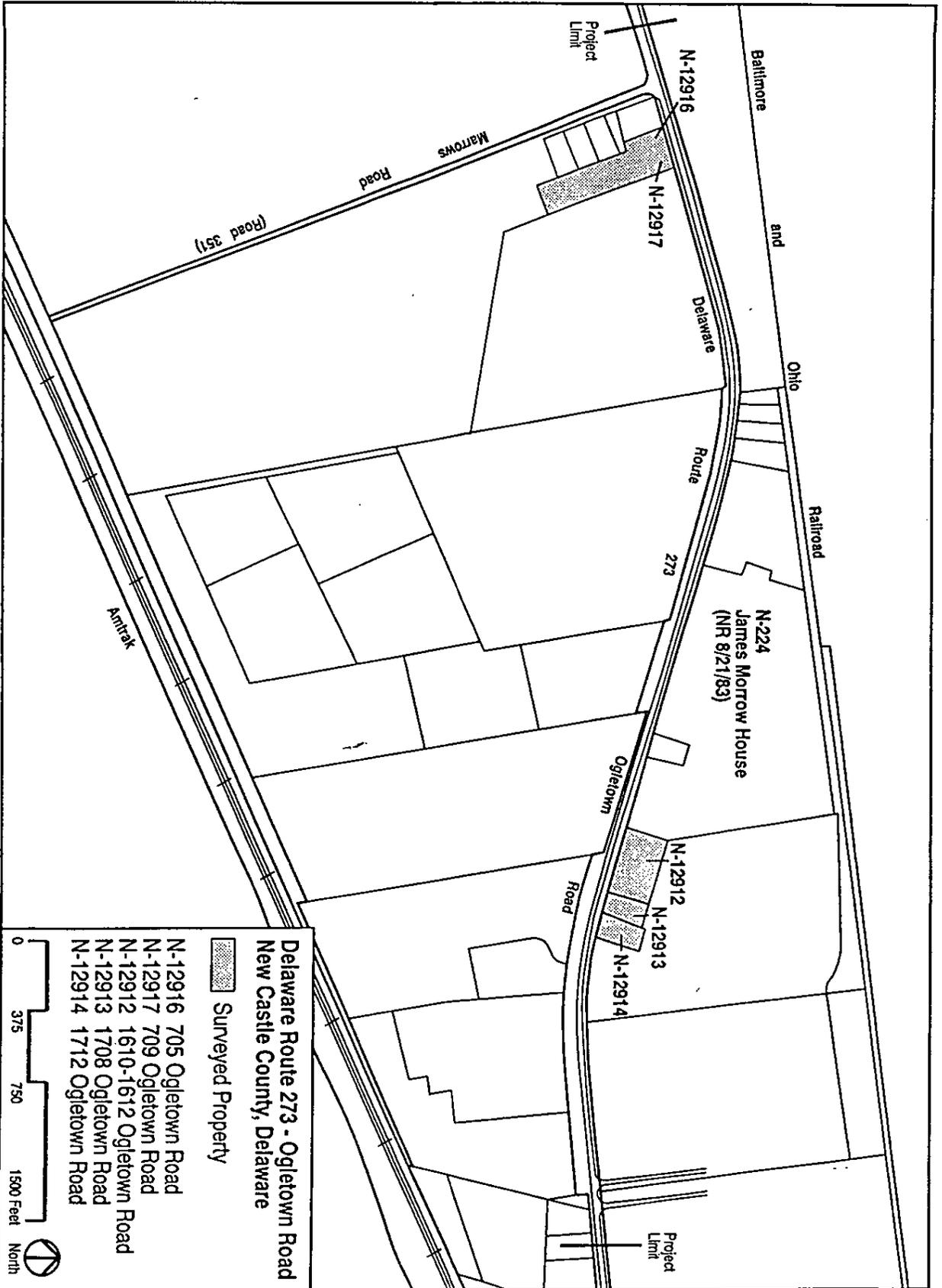


Figure 3. Resource Location Map

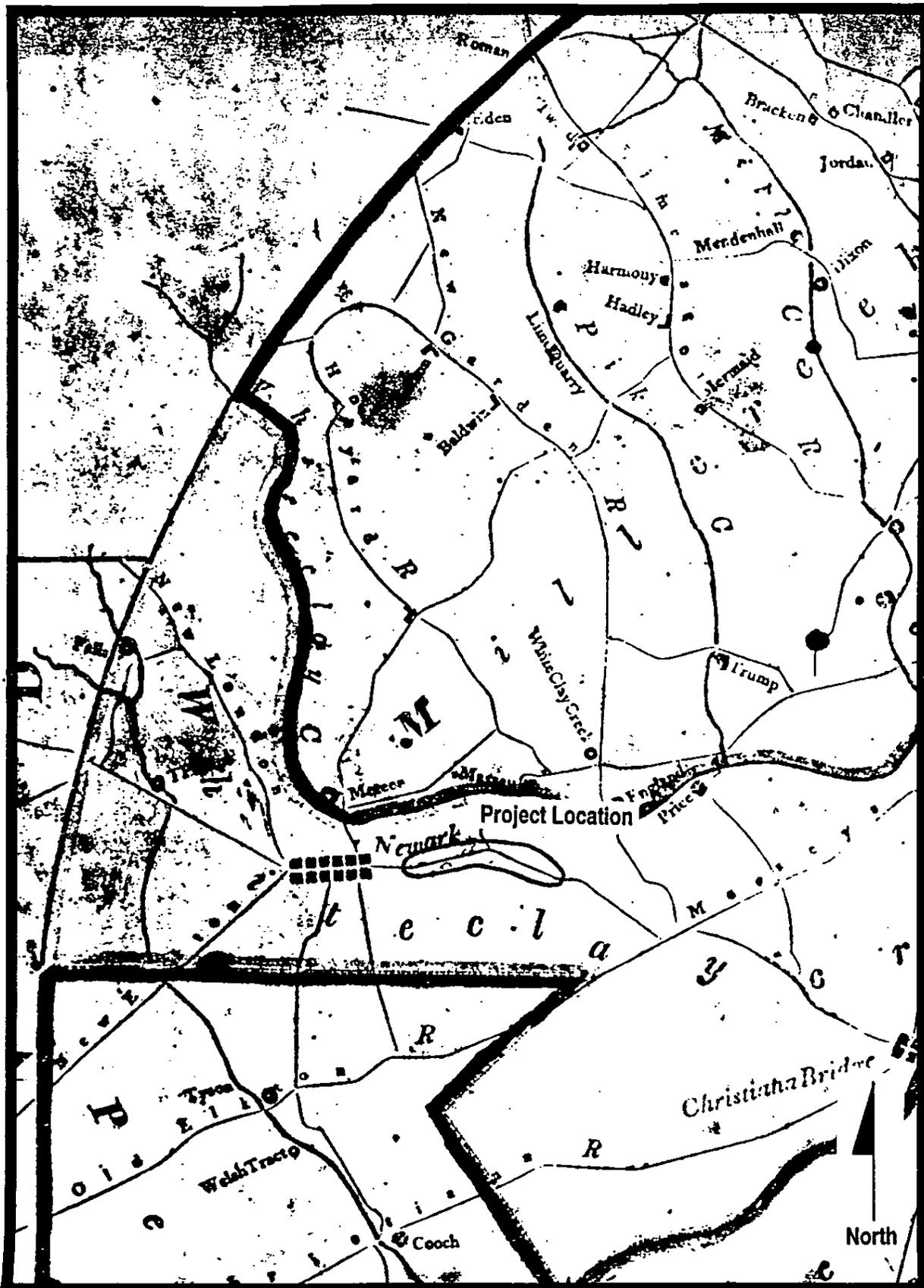
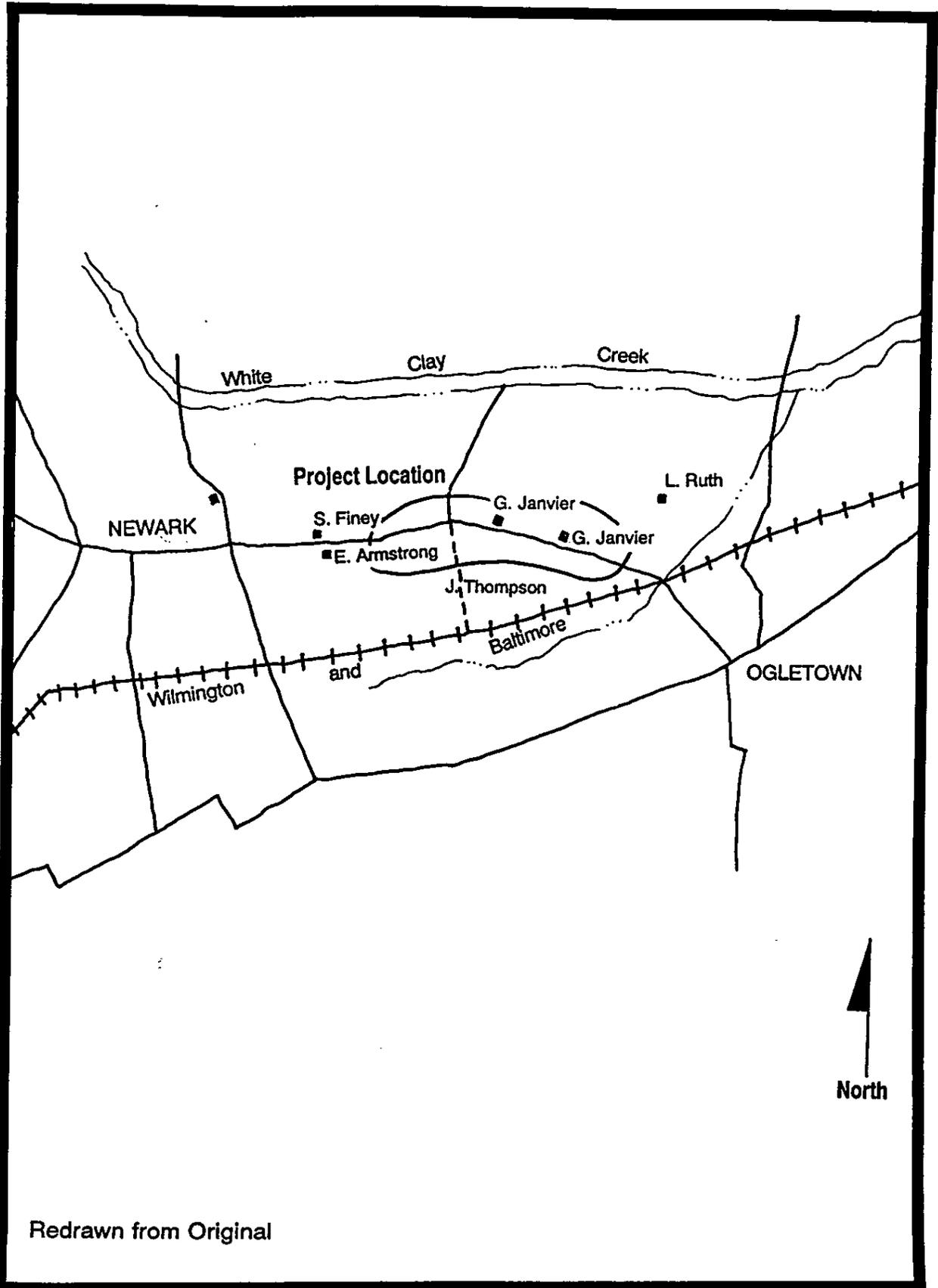


Figure 4. Henry Heald, *Roads of New Castle County* (1820).
Scale: 1 inch = 3/4 mile



Redrawn from Original

Figure 5. S.M. Rea & J. Price, *Map of New Castle, Delaware* (1849).
Scale: 1 3/4 inch = 1 mile

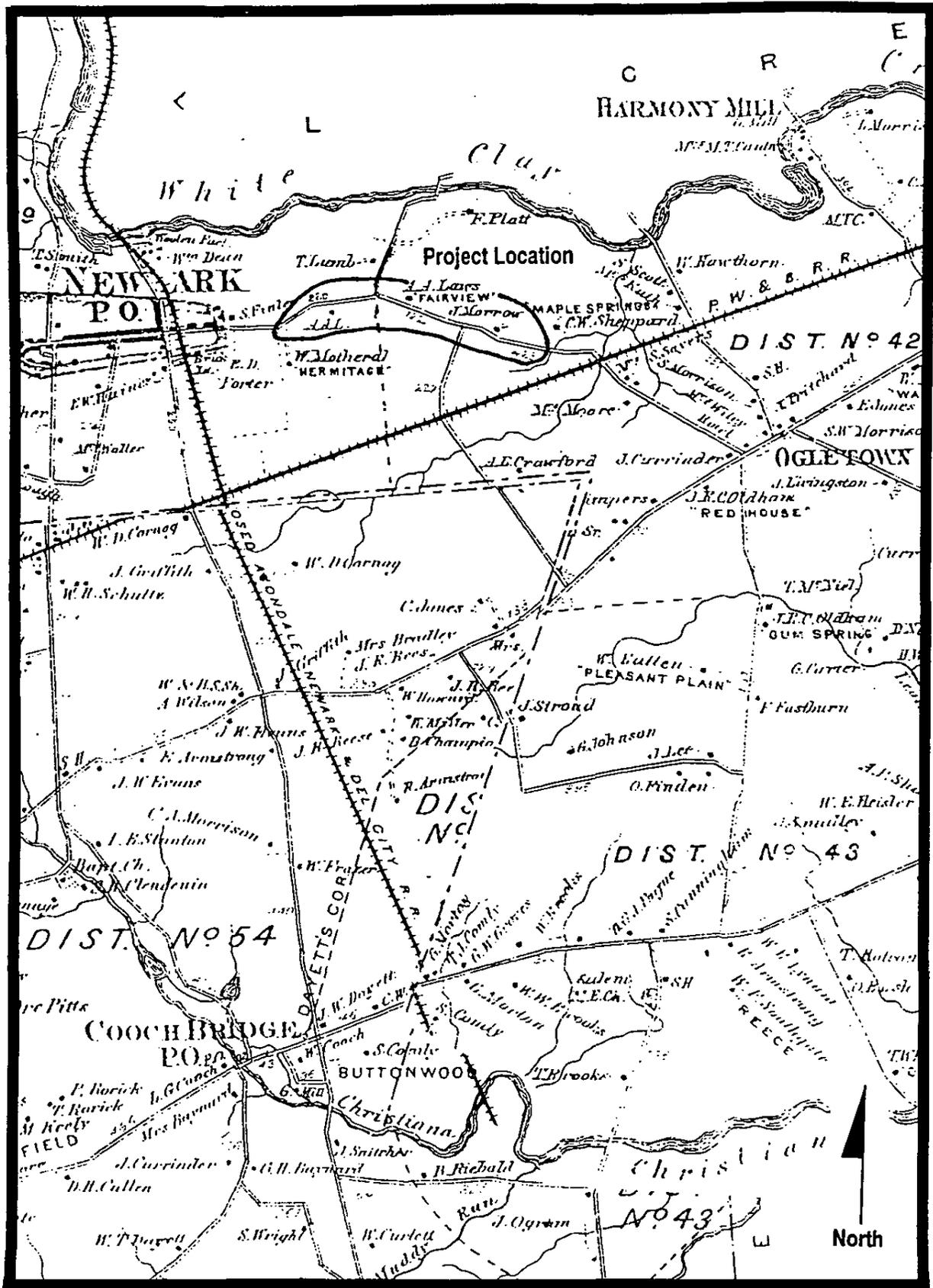


Figure 6. Pomeroy & Beers, Atlas of the State of Delaware (1868).
 Scale: 1 3/4 inch = 1 mile



Figure 7. G.M. Hopkins & Co., Map of New Castle County, Delaware (1881).
 Scale: 1 3/4 inch = 1 mile

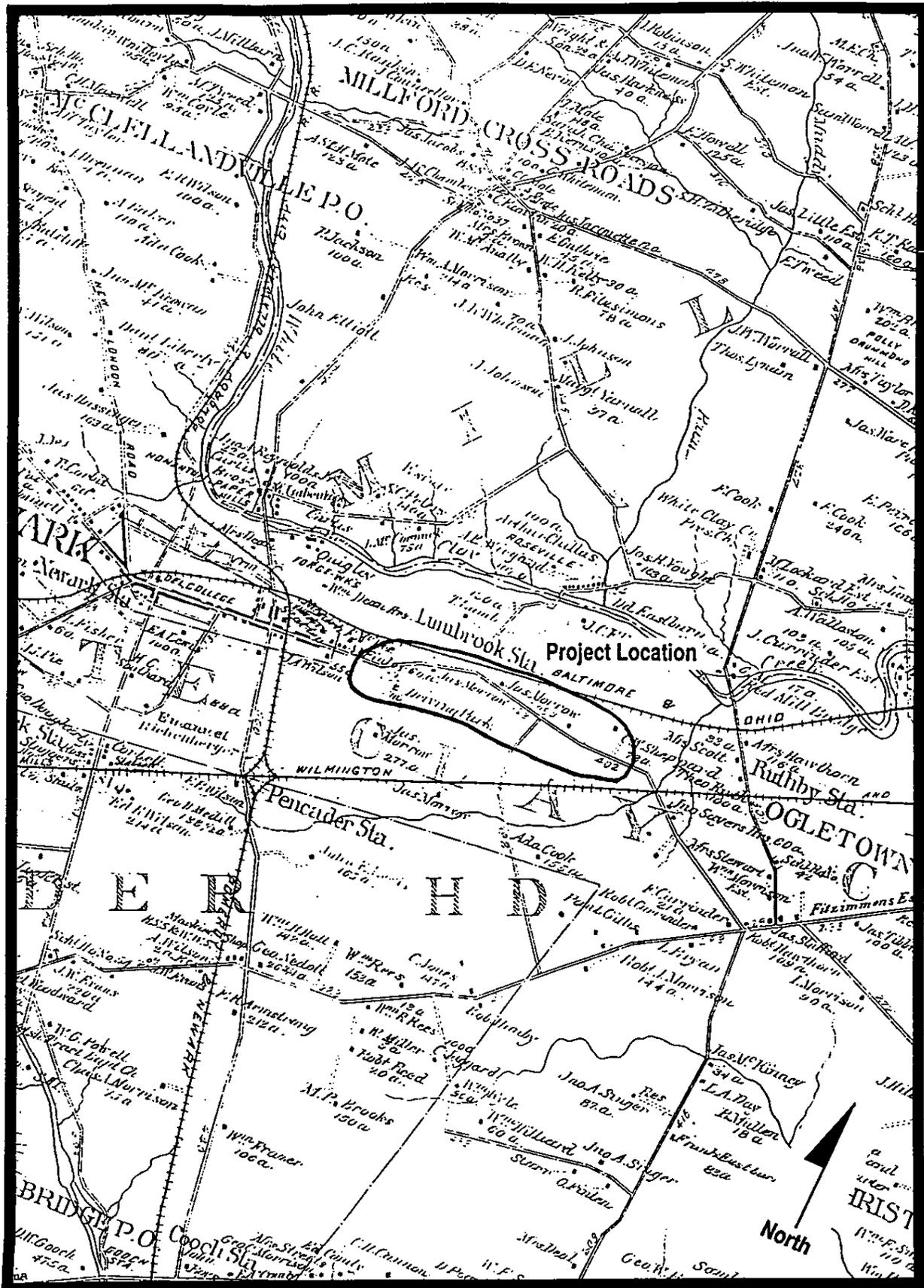


Figure 8. Wm. G. Baist, Atlas of New Castle County, Delaware (1893).
 Scale: 1 3/4 inch = 1 mile

APPENDIX A



CULTURAL RESOURCE SURVEY
 CONSTRUCTION DATA FORM

FORM CRS-1

CRS no.	<u>N-12916</u>
SPO Map	<u>06-07-34</u>
Hundred	<u>Wh. Clay Crk.</u>
Quad	<u>Newark East</u>
Zone	<u>I</u>
Acreage	<u>2.13</u>

1. ADDRESS OF PROPERTY: 705 Ogletown Road, Newark, DE 19711
2. DATE OF INITIAL CONSTRUCTION: circa 1925
3. STYLE/FLOOR PLAN: Foursquare
4. ARCHITECT/BUILDER: unknown
5. INTEGRITY: a) original site b) moved _____
 c) if moved, when and from where _____
 d) list major alterations and dates (if known)

6. CONDITION: good deteriorated _____
 remarks: The dwelling retains a majority of its original architectural features; A modern garage is located south of the house.

7. DESCRIBE THE RESOURCE AS COMPLETELY AS POSSIBLE:

- a) Overall shape

stories	square; two-and-one-half
bays	two
wings	none
- b) Structural system masonry
- c) Foundation

materials	molded concrete block
basement	yes
- d) Exterior walls (modern over original)

materials	common bond brick
color(s)	red
- e) Roof

shape; materials	hipped with intersecting rear shed; asphalt shingles, german siding in gable
cornice	exposed rafters in eaves and aluminum gutters
dormers	north (front) facade - hipped with paired four light vertical sash windows,
chimney location(s)	german siding and exposed rafters in eaves. Chimney - Center roof slope, brick with simple cap.

7. DESCRIPTION (cont'd):

CRS # N-12916

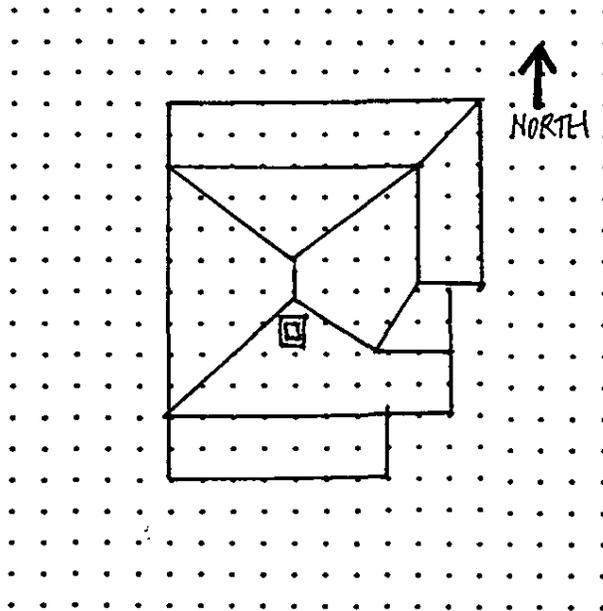
- f) Windows
 - spacing regular
 - type single and paired, three-over-one
 - trim wood surround, simple wood sills and soldier arch brick lintels
 - shutters none

- g) Door
 - spacing primary - west bay of north facade; secondary - east bay of north facade
 - type wood, single leaf, fifteen light
 - trim simple wood surround, soldier arch brick lintels

- h) Porches
 - location(s) north and east facades, wrap-around
 - materials wood, brick, asphalt shingle roof
 - supports brick piers with molded stone caps support square battered wood columns with wood bases and caps. Trim - lattice infill below porch, wood balustrade with square posts, railing, exposed rafters in eaves, blank wood frieze, and aluminum gutters.
 - trim

- i) Interiors details (if accessible)
 - Not accessible

8. SKETCH PLAN OF BUILDING:



INDICATE NORTH ON SKETCH

9. Surveyor: Jill Cremer and Martin B. Abbot Date of Form May 1993



CULTURAL RESOURCE SURVEY
PROPERTY IDENTIFICATION FORM

FORM CRS-3

CRS no.	<u>N-12916</u>
SPO Map	<u>06-07-34</u>
Hundred	<u>Wh. Clay Cr.</u>
Quad	<u>Newark East</u>
Zone	<u>1</u>
Acreage	<u>2.13</u>

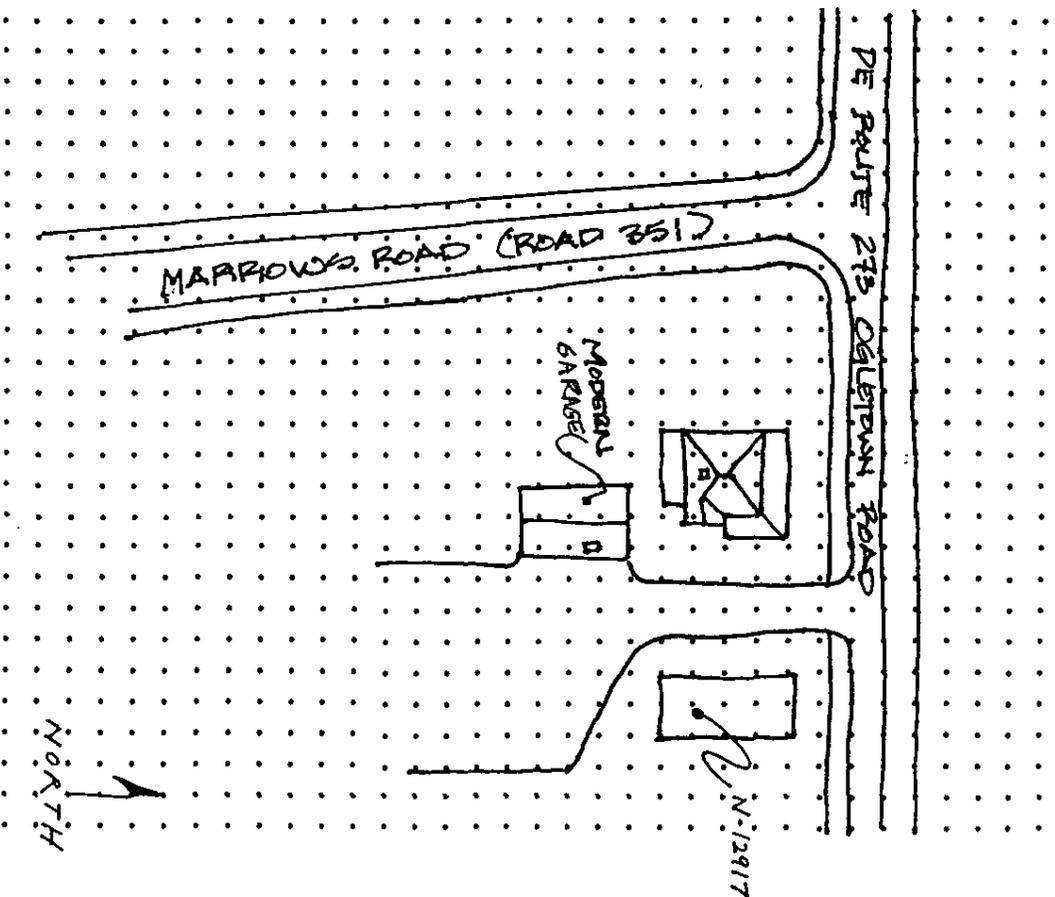
1. NAME OF PROPERTY: 705 Ogletown Road
2. STREET LOCATION: 705 Ogletown Road, Newark, DE 19711
3. OWNER'S NAME: Robert Tolliver TEL. #: not available
ADDRESS: 709 Ogletown Road, Newark, DE 19711
4. TYPE OF RESOURCE: a) building x b) structure _____ c) site _____
d) object _____ e) district _____ f) other _____
5. SURROUNDINGS: (check more than one if necessary)
a) fallow field _____ b) cultivated field _____ c) woodland _____
d) scattered buildings x e) densely built up _____ f) other _____
6. FUNCTION: original Single Family Residential present Single Family Residential
7. LIST ADDITIONAL SHEETS USED
None

8. SURVEYOR: Jill Cremer and Martin B. Abbot TEL. # 215-561-1050
ORGANIZATION: Kise Franks & Straw, Inc. DATE: May 1993
ADDRESS: 219 N. Broad Street, Philadelphia, PA 19107

9. SKETCH MAP

CRS # N-12916

Please indicate position of resource in relation to geographical landmarks such as streams and roads.



INDICATE NORTH ON SKETCH

USE BLACK INK ONLY

10. CONTEXT Consider the following:

- a) relationship to setting
- b) associated traditions or stories
- c) noteworthy features
- d) comparison with others in area
- e) threats
- f) additional documentation

New Castle County property records indicate that Samuel E. Dameron purchased this parcel from Julia Huber in October 1924. Dameron subdivided the property and this Foursquare-style dwelling was built circa 1925. Features of the Foursquare style exhibited at 705 Ogletown Road include the box form, the low hipped roof with exposed rafters, the wraparound porch with brick piers and wood columns, and the three-over-one paired windows. In 1963, this parcel was consolidated with the adjacent property at 709 Ogletown Road (N-12917) to form one property. Although this resource retains a majority of its original architectural features and is in good condition, it is a typical rather than innovative example of the Foursquare style. In addition, the resource is not associated with a larger residential subdivision and therefore, does not warrant historic significance as a suburban dwelling.

11. Comprehensive Planning:

- a. Time Period(s) 1880-1940 +/-
- b. Historic Themes(s) Settlement Patterns and Demo. Change

12. Evaluation and Eligibility

- a) area(s) of significance Appears Not Eligible
- b) criteria

13. Certification

PI: Name/Title Jill Cremer and Martin B. Abbot Date May 1993
 SHPO: Name/Title _____ Date _____

PHOTOGRAPHIC INVENTORY

BUREAU OF ARCHAEOLOGY AND HISTORIC PRESERVATION

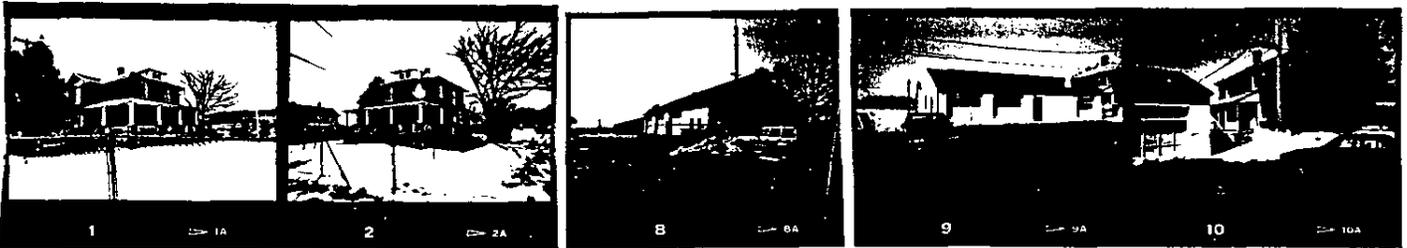
Document 20-06/79/01/11

CRS Number N-12916 Date MARCH 1993 Contact Sheet # _____

Description 1 - DWELLING, VIEW SOUTHWEST. 2 - DWELLING, VIEW SOUTHEAST. 8 - MODERN GARAGE, VIEW SOUTH-SOUTHWEST. 9 - SAME, VIEW WEST. 10 - SAME, VIEW NORTHWEST.

Location (if other than Bureau collection) _____

Attach contact print





CULTURAL RESOURCE SURVEY
 CONSTRUCTION DATA FORM

FORM CRS-1

CRS no.	<u>N-12917</u>
SPO Map	<u>06-07-34</u>
Hundred	<u>Wh. Clay Cr.</u>
Quad	<u>Newark East</u>
Zone	<u>I</u>
Acreage	<u>2.13</u>

1. ADDRESS OF PROPERTY: 709 Ogletown Road, Newark, DE 19711
2. DATE OF INITIAL CONSTRUCTION: circa 1910
3. STYLE/FLOOR PLAN: Vernacular
4. ARCHITECT/BUILDER: unknown

5. INTEGRITY: a) original site _____ b) moved _____
- c) if moved, when and from where _____
- d) list major alterations and dates (if known)
According to present owner, the dwelling was stuccoed circa 1984. Aluminum trim was also applied to the window surrounds and eaves.

6. CONDITION: good _____ deteriorated _____

remarks: _____

7. DESCRIBE THE RESOURCE AS COMPLETELY AS POSSIBLE:

- a) Overall shape

stories	rectangular; two-and-one-half
bays	two
wings	south facade one story frame shed addition, aluminum sided
- b) Structural system frame
- c) Foundation

materials	rusticated concrete block; shed addition - modern concrete block
basement	yes; earlier stone foundation in basement
- d) Exterior walls (modern over original)

materials	stucco
color(s)	white
- e) Roof

shape; materials	intersecting gables; asphalt shingles
cornice	boxed and covered with aluminum
dormers	none
chimney location(s)	west roof slope of south (rear) intersecting gable - brick with simple cap

7. DESCRIPTION (cont'd):

CRS # N-12917

f) Windows

spacing	regular
type	two-over-two original and replacement double hung wood sash
trim	aluminum surround
shutters	none

g) Door

spacing	primary - west bay of north (front) facade; secondary - center bay of west facade
type	single leaf, nine-light; single leaf, modern replacement
trim	simple wood

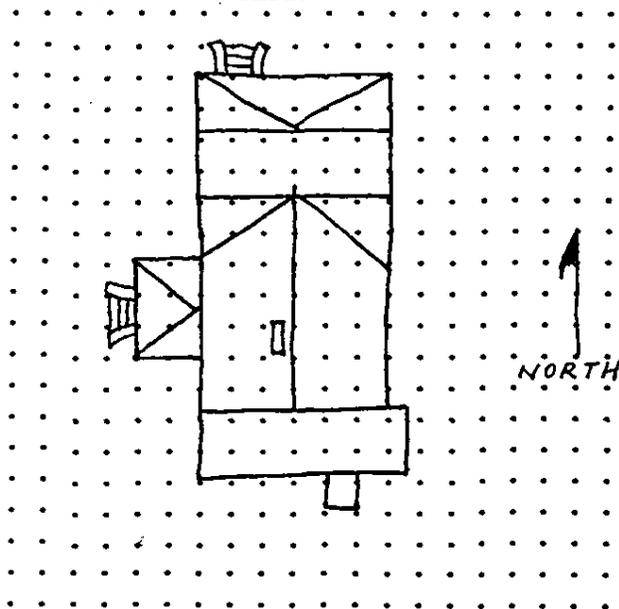
h) Porches

location(s)	north (front) and west facades
materials	wood, asphalt shingle roof
supports	three wood round columns each with molded base and cap on north and west facades
trim	aluminum gutters and concrete steps with flared and stuccoed side walls

i) Interiors details (if accessible)

not accessible

8. SKETCH PLAN OF BUILDING:



INDICATE NORTH ON SKETCH

9. Surveyor: Mark A. Bower and Martin B. Abbot

Date of Form December 1994



CULTURAL RESOURCE SURVEY
PROPERTY IDENTIFICATION FORM

FORM CRS-3

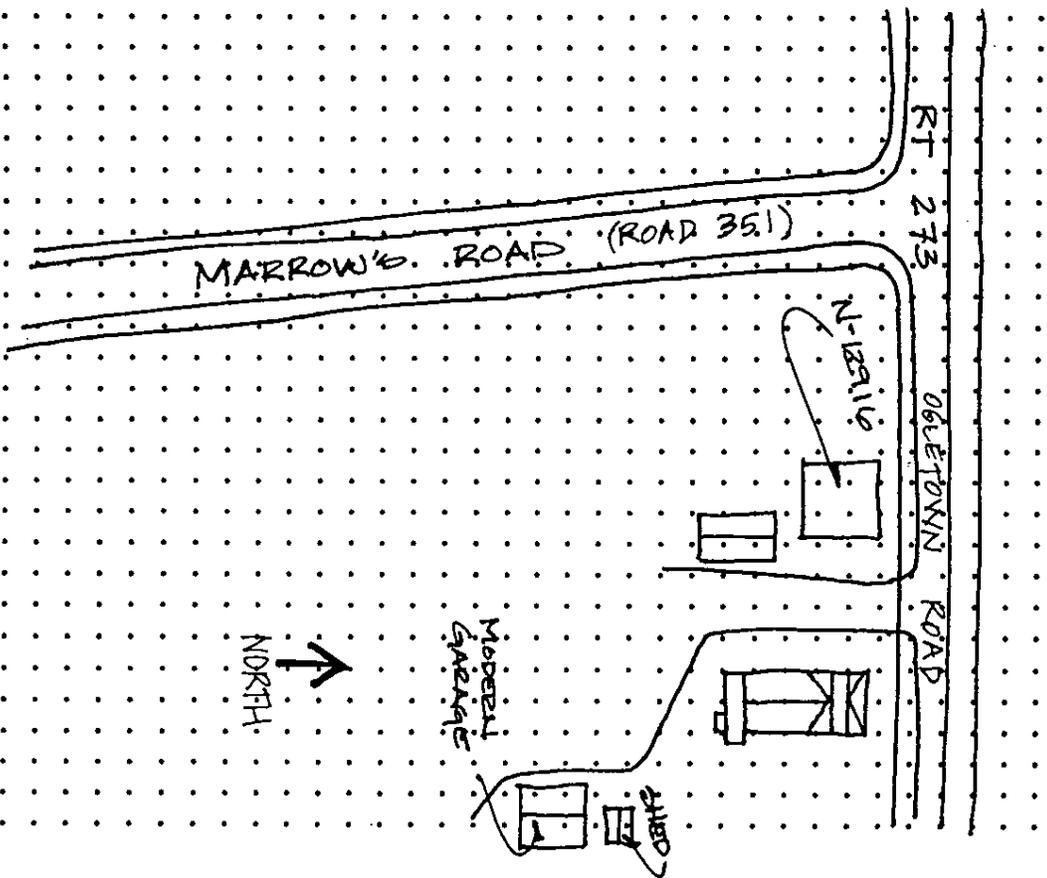
CRS no.	<u>N-12917</u>
SPO Map	<u>06-07-34</u>
Hundred	<u>Wh. Clay Cr.</u>
Quad	<u>Newark East</u>
Zone	<u>I</u>
Acreage	<u>2.13</u>

1. NAME OF PROPERTY: 709 Ogletown Road
2. STREET LOCATION: 709 Ogletown Road, Newark, DE 19711
3. OWNER'S NAME: Robert Tolliver TEL. #: not available
ADDRESS: 709 Ogletown Road, Newark, DE 19711
4. TYPE OF RESOURCE: a) building x b) structure _____ c) site _____
d) object _____ e) district _____ f) other _____
5. SURROUNDINGS: (check more than one if necessary)
a) fallow field _____ b) cultivated field _____ c) woodland _____
d) scattered buildings x e) densely built up _____ f) other _____
6. FUNCTION: original Single Family Residential present Commercial
7. LIST ADDITIONAL SHEETS USED
CRS-7 Related Outbuildings Form (1)
8. SURVEYOR: Mark A. Bower and Martin B. Abbot TEL. # 215-561-1050
ORGANIZATION: Kise Franks & Straw, Inc. DATE: December 1994
ADDRESS: 219 N. Broad Street, Philadelphia, PA 19107

9. SKETCH MAP

CRS # N-12917

Please indicate position of resource in relation to geographical landmarks such as streams and roads.



INDICATE NORTH ON SKETCH

USE BLACK INK ONLY

10. CONTEXT Consider the following:

- a) relationship to setting
- b) associated traditions or stories
- c) noteworthy features
- d) comparison with others in area
- e) threats
- f) additional documentation

It appears this dwelling was constructed circa 1910 for use as a farm dwelling. The property was owned by the Morrow Family, proprietors of the James Morrow House (N-224), from 1875 to 1909 when a 3-acre tract containing a frame dwelling house was sold to Wm. J. Lovett of Philadelphia for \$1075. Thirteen months later, Lovett sold the same tract to Chas. R. Thomas for more than twice his purchase price (\$2200). The 1920 census listed Thomas as a farmer of his "own accounts." Interior inspection of the dwelling revealed the presence of an earlier stone foundation under the existing rusticated concrete block foundation. Due to the increase in price and the early 20th-century details of the dwelling, such as the foundation, standard-width wood flooring, and front and side porch details, it appears Lovett most likely removed the frame dwelling noted in his deed and erected the present dwelling over the former's foundation. In 1963, this parcel was consolidated with the adjacent property at 705 Ogletown Road (N-12916) to form one property.

Post-1950 alterations, including the application of stucco siding and aluminum window trim, have severely compromised the architectural integrity of the resource. Also, the resource's original agricultural setting is no longer extant.

11. Comprehensive Planning:

- a. Time Period(s) 1880-1940+/-
- b. Historic Themes(s) Agriculture

12. Evaluation and Eligibility

- a) area(s) of significance Appears Not Eligible
- b) criteria

13. Certification

PI: Name/Title Mark Bower and Martin Abbot Date 12/1994
 SHPO: Name/Title _____ Date _____



CULTURAL RESOURCE SURVEY
RELATED OUTBUILDINGS FORM

FORM CRS-7

CRS no.	<u>N-12917</u>
SPO Map	<u>06-07-34</u>
Hundred	<u>Wh. Clay Cr.</u>
Quad	<u>Newark East</u>
Zone	<u>1</u>
Acreage	<u>2.13</u>

1. ADDRESS OF PROPERTY: 709 Ogletown Road, Newark; DE 19711
2. FUNCTION: Storage Shed
3. DATE: early twentieth century
4. STYLISTIC FEATURES: simple one bay square
5. ARCHITECT/BUILDER: unknown
6. DESCRIPTION:
 - a) Structural System one story frame over above-ground cellar
 - b) Wall Coverings aluminum siding over frame German siding
 - c) Wall Openings
 windows one-over-one double hung aluminum replacement sash
 doors center bay west facade located above ground level; three lights over three horizontal
 other panels; none
 - d) Foundation concrete footings
 - e) Roof
 structural system frame, front gabled
 coverings asphalt shingles
 openings none
 - f) Interiors
 floor Plan
 open

 partition/walls
 none

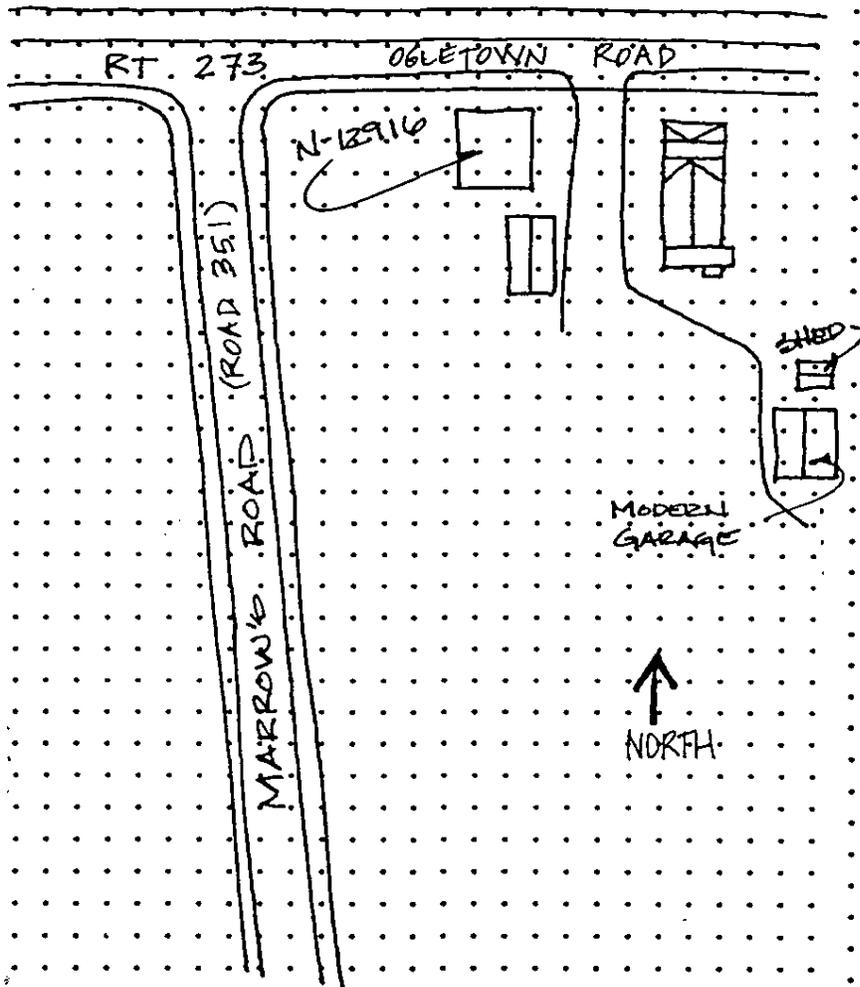
 interior finish
 exposed beams

 furnishings/machinery
 none

7. SKETCH MAP

CRS # N-12917

Show relationship to main building and provide sketch plan of outbuilding.



INDICATE NORTH ON SKETCH

8. SURVEYOR: Mark A. Bower and Martin B. Abbot DATE OF FORM: December 1994

PHOTOGRAPHIC INVENTORY

BUREAU OF ARCHAEOLOGY AND HISTORIC PRESERVATION

Document 20-06/79/01/11

CRS Number N-12917 Date MARCH 1993 Contact Sheet # _____

Description 3- VIEW EAST ALONG RT. 273 SHOWING RESIDUE ON PUGH. 4- DWELLING VIEW SOUTH. 5- DWELLING, VIEW NORTH-NORTHEAST

Location (if other than Bureau collection) _____

Attach contact print



PHOTOGRAPHIC INVENTORY

BUREAU OF ARCHAEOLOGY AND HISTORIC PRESERVATION

Document 20-06/79/01/11

CRS Number N-12917 Date MARCH 1993 Contact Sheet # _____

Description 6- OUTBUILDINGS, VIEW SOUTHEAST. 7- DWELLING, VIEW NORTHWEST.

Location (if other than Bureau collection) _____

Attach contact print





CULTURAL RESOURCE SURVEY
CONSTRUCTION DATA FORM

FORM CRS-1

CRS no.	N-12912
SPO Map	06-07-34
Hundred	Wh. Clay Cr.
Quad	Newark East
Zone	1
Acreage	1.37

1. ADDRESS OF PROPERTY: 1610-1612 Ogletown Road, Newark, DE 19711
2. DATE OF INITIAL CONSTRUCTION: circa 1936
3. STYLE/FLOOR PLAN: Foursquare
4. ARCHITECT/BUILDER: unknown
5. INTEGRITY: a) original site _____ b) moved _____
c) if moved, when and from where _____
d) list major alterations and dates (if known)

6. CONDITION: good _____ deteriorated _____
remarks: _____

7. DESCRIBE THE RESOURCE AS COMPLETELY AS POSSIBLE:
 - a) Overall shape
stories square; two-and-one-half
bays four
wings north (rear) facade enclosed shed porch
 - b) Structural system masonry
 - c) Foundation
materials stucco over concrete block
basement yes
 - d) Exterior walls (modern over original)
materials common bond brick
color(s) red
 - e) Roof
shape; materials side-gabled with pediments in east and west gables; asphalt shingles
cornice box with plain fascia board
dormers two on south (front) facade, pedimented with paired one-over-one double
chimney location(s) hung windows, asbestos shingle sides and asphalt shingle roof. Chimney -
north roof slope, brick.

7. DESCRIPTION (cont'd):

CRS # N-12912

f) Windows

spacing regular
type single and paired, one-over-one double hung
trim simple wood surround with brick sills and aluminum awnings
shutters no shutters

g) Door

spacing south (front) facade, east and west bays
type single leaf, nine light
trim molded wood

h) Porches

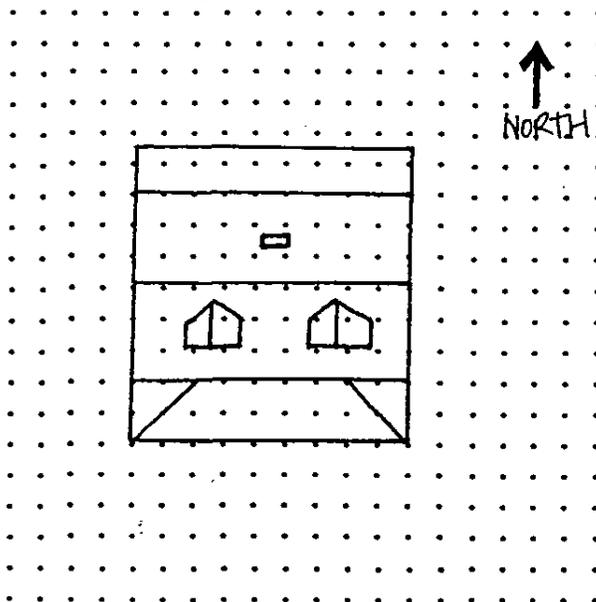
location(s) south (front) facade
materials wood and brick
supports brick piers with molded stone caps support wood Tuscan columns
trim concrete steps with brick side walls and stone caps at east and west ends, lattice infill between piers below porch, wood balustrade and railing, box cornice, blank frieze,

i) Interiors details (if accessible)

not accessible

asphalt shingle roof

8. SKETCH PLAN OF BUILDING:



INDICATE NORTH ON SKETCH

9. Surveyor: Jill Cremer and Martin B. Abbot

Date of Form May 1993



CULTURAL RESOURCE SURVEY
PROPERTY IDENTIFICATION FORM

FORM CRS-3

CRS no.	<u>N-12912</u>
SPO Map	<u>06-07-34</u>
Hundred	<u>Wh. Clay Cr.</u>
Quad	<u>Newark East</u>
Zone	<u>1</u>
Acreage	<u>1.37</u>

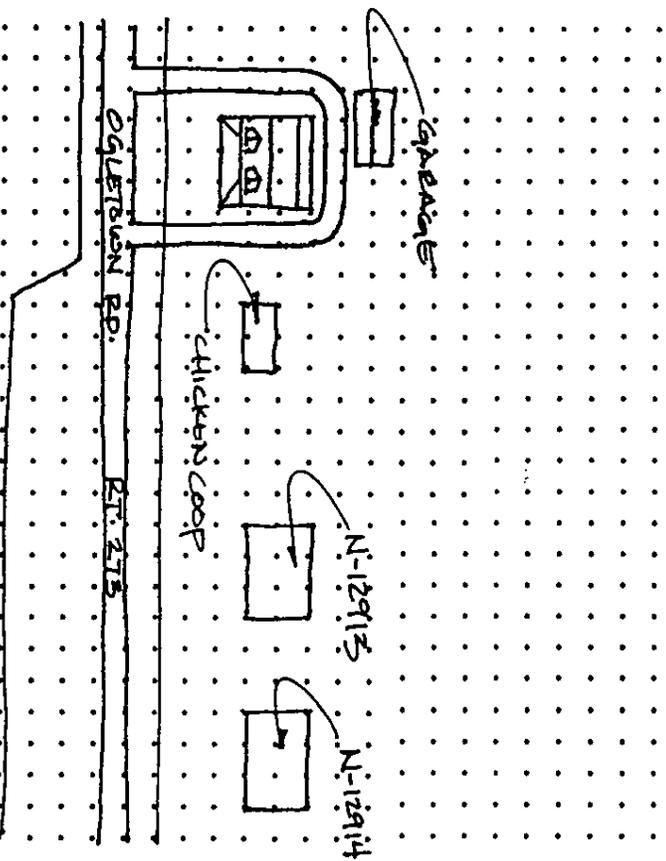
1. NAME OF PROPERTY: 1610-1612 Ogletown Road
2. STREET LOCATION: 1610-1612 Ogletown Road, Newark, DE 19711
3. OWNER'S NAME: William N. Wilhelm TEL. #: 302-737-6742
ADDRESS: 1612 Ogletown Road, Newark, DE 19711
4. TYPE OF RESOURCE: a) building b) structure _____ c) site _____
d) object _____ e) district _____ f) other _____
5. SURROUNDINGS: (check more than one if necessary)
a) fallow field _____ b) cultivated field _____ c) woodland _____
d) scattered buildings e) densely built up _____ f) other _____
6. FUNCTION: original Two Family Residential present Two Family Residential
7. LIST ADDITIONAL SHEETS USED
CRS-7 Related Outbuildings Form (2)

8. SURVEYOR: Jill Cremer and Martin B. Abbot TEL. # 215-561-1050
ORGANIZATION: Kise Franks & Straw, Inc. DATE: May 1993
ADDRESS: 219 N. Broad Street, Philadelphia, PA 19107

9. SKETCH MAP

CRS # N-12912

Please indicate position of resource in relation to geographical landmarks such as streams and roads.



INDICATE NORTH ON SKETCH

USE BLACK INK ONLY

10. CONTEXT Consider the following:

- a) relationship to setting
- b) associated traditions or stories
- c) noteworthy features
- d) comparison with others in area
- e) threats
- f) additional documentation

This property was purchased by John F. Richards in 1911 as part of a 300 acre tract of land. Prior to the erection of this dwelling, the land was part of five acres that Richards separated from his 300 acre farm, and subsequently rented to farmers. The five acre parcel contained a house (destroyed by fire circa 1925), one brick barn, one stone barn, and a chicken coop. The only extant resource is the chicken coop, which remains east of the house. According to Richards' grandson, Robert Stafford, this dwelling and garage were constructed circa 1936. Bricks salvaged from a barn on the adjacent property, which is the present-day location of 1708 Ogletown Road (N-12913), were used to lay up the walls which were then clad with the present veneer brick. After Richards' death in 1947, his granddaughter and her husband, Emilie and William Wilhelm, acquired the property. Features of the style exhibited at 1610-1612 Ogletown Road include the box form and the full-width front porch. Although this resource retains many of its original architectural features and remains relatively unaltered, it is a typical rather than innovative example of the Foursquare style. In addition, the resource is not associated with a larger residential subdivision and therefore, does not warrant historic significance as a suburban dwelling.

11. Comprehensive Planning:

- a. Time Period(s) 1880-1940 +/-
- b. Historic Themes(s) Settlement Patterns and Demo. Change

12. Evaluation and Eligibility

- a) area(s) of significance Appears Not Eligible
- b) criteria

13. Certification

PI: Name/Title Jill Cremer and Martin B. Abbot Date May 1993
 SHPO: Name/Title _____ Date _____



CULTURAL RESOURCE SURVEY
RELATED OUTBUILDINGS FORM

FORM CRS-7

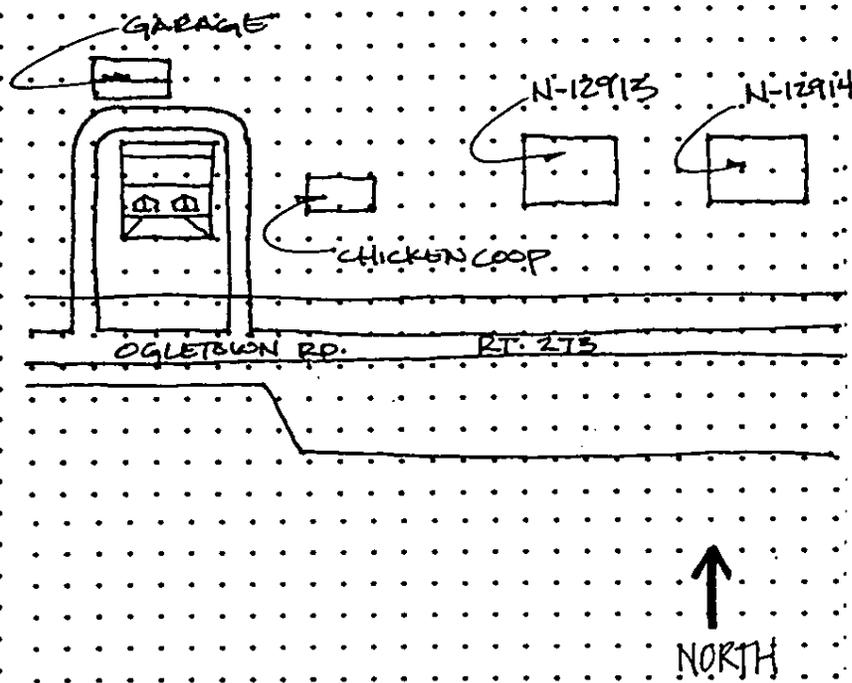
CRS no.	<u>N-12912</u>
SPO Map	<u>06-07-34</u>
Hundred	<u>Wh. Clay Cr.</u>
Quad	<u>Newark East</u>
Zone	<u>1</u>
Acreage	<u>1.37</u>

1. ADDRESS OF PROPERTY: 1610-1612 Ogletown Road, Newark, DE 19711
2. FUNCTION: Chicken Coop (This building is historically associated with an earlier farmhouse, now demolished, located on the site of present-day 1708 Ogletown Road, N-12913)
3. DATE: circa 1900
4. STYLISTIC FEATURES: typical coop form, low, long building with single slop roof
5. ARCHITECT/BUILDER: unknown
6. DESCRIPTION:
 - a) Structural System one story, frame
 - b) Wall Coverings asphalt shingles, with wood corner boards
 - c) Wall Openings
 - windows paired one-over-one double hung, and tripled with single fixed lights
 - doors single leaf, west facade
 - other none
 - d) Foundation concealed by shingles
 - e) Roof
 - structural system frame, single slope
 - coverings asphalt paper with plain fascia board
 - openings none
 - f) Interiors
 - floor Plan not accessible
 - partition/walls not accessible
 - interior finish not accessible
 - furnishings/machinery not accessible

7. SKETCH MAP

CRS # N-12912

Show relationship to main building and provide sketch plan of outbuilding.



INDICATE NORTH ON SKETCH

8. SURVEYOR: Jill Cremer and Martin B. Abbot

DATE OF FORM: May 1993



CULTURAL RESOURCE SURVEY
RELATED OUTBUILDINGS FORM

FORM CRS-7

CRS no.	N-12912
SPO Map	06-07-34
Hundred	Wh. Clay Cr.
Quad	Newark East
Zone	I
Acreage	1.37

1. ADDRESS OF PROPERTY: 1610-1612 Ogletown Road, Newark, DE 19711
2. FUNCTION: Garage
3. DATE: circa 1936
4. STYLISTIC FEATURES: one bay garage - open plan
5. ARCHITECT/BUILDER: unknown
6. DESCRIPTION:
 - a) Structural System concrete block, one story
 - b) Wall Coverings concrete block, painted white
 - c) Wall Openings
 - windows within sliding cross braced garage doors
 - doors sliding vertical board and cross braced garage door on south and east facades with paired,
 - other fixed, six-light sash. no other.
 - d) Foundation concrete block
 - e) Roof
 - structural system frame, side gabled with plain fascia board
 - coverings asphalt shingle
 - openings none
 - f) Interiors
 - floor Plan not accessible

 - partition/walls not accessible

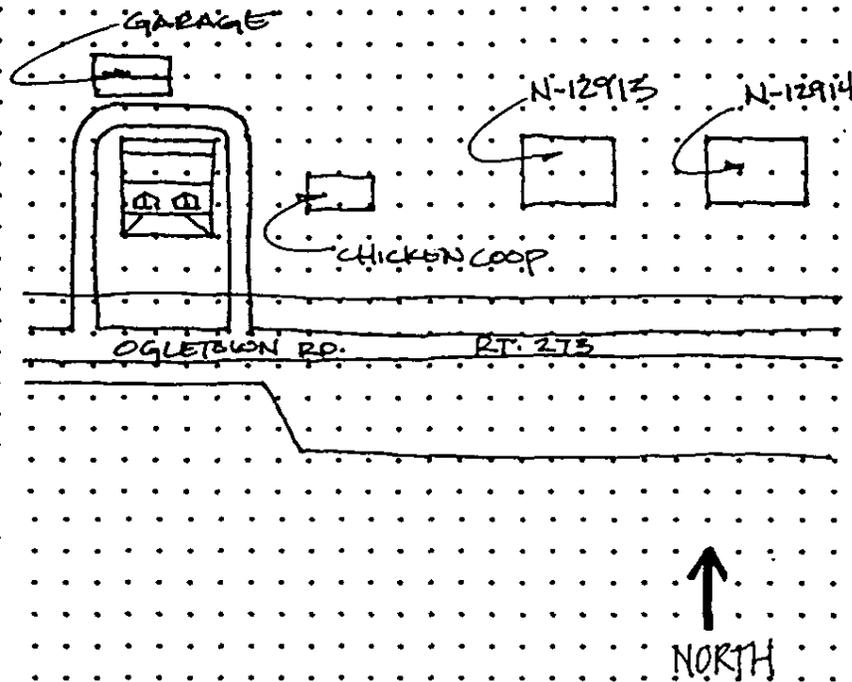
 - interior finish not accessible

 - furnishings/machinery not accessible

7. SKETCH MAP

CRS # N-12912

Show relationship to main building and provide sketch plan of outbuilding.



INDICATE NORTH ON SKETCH

8. SURVEYOR: Jill Cremer and Martin B. Abbot

DATE OF FORM: May 1993

PHOTOGRAPHIC INVENTORY

BUREAU OF ARCHAEOLOGY AND HISTORIC PRESERVATION

Document 20-06/79/01/11

CRS Number N-12912 Date MARCH 1993 Contact Sheet # _____

Description 11 - DWELLING, VIEW NORTHEAST. 12 - SAME. 13 - DWELLING AND GARAGE, VIEW NORTH-NORTHEAST.

Location (if other than Bureau collection) _____

Attach contact print



PHOTOGRAPHIC INVENTORY

BUREAU OF ARCHAEOLOGY AND HISTORIC PRESERVATION

Document 20-06/79/01/11

CRS Number N-12912 Date MARCH 1993 Contact Sheet # _____

Description 14 - DWELLING, VIEW NORTHWEST. 15 - SAME.

16 - VIEW WEST ALONG RT. 293 - RESOURCE ON RIGHT.

Location (if other than Bureau collection) _____

Attach contact print



PHOTOGRAPHIC INVENTORY

BUREAU OF ARCHAEOLOGY AND HISTORIC PRESERVATION

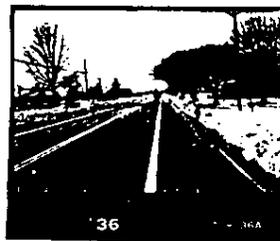
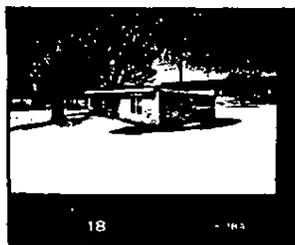
Document 20-06/79/01/11

CRS Number N-12912 Date MARCH 1993 Contact Sheet # _____

Description 18 - CHICKEN COOP, VIEW NORTHWEST. 33 - VIEW WEST
ALONG RT. 273 - RESOURCE ON RIGHT. 36 - VIEW WEST ALONG RT. 273 -
RESOURCE ON RIGHT.

Location (if other than Bureau collection) _____

Attach contact print





CULTURAL RESOURCE SURVEY
CONSTRUCTION DATA FORM

FORM CRS-1

CRS no.	N-12913
SPO Map	06-07-34
Hundred	Wh. Clay Cr.
Quad	Newark East
Zone	
Acreage	.48

1. ADDRESS OF PROPERTY: 1708 Ogletown Road, Newark, DE 19711

2. DATE OF INITIAL CONSTRUCTION: circa 1926

3. STYLE/FLOOR PLAN: Foursquare

4. ARCHITECT/BUILDER: unknown

5. INTEGRITY: a) original site b) moved _____
c) if moved, when and from where _____

d) list major alterations and dates (if known)
This dwelling was constructed in the same location as an earlier house that was destroyed in a circa 1925 fire. The current owner claims that a cornerstone inscribed "1756" was found under the original porch of the present dwelling.

6. CONDITION: good _____ deteriorated _____

remarks: This resource is in fair condition. It appears that a majority of the historic architectural features have been concealed or removed through post-1950's alterations.

7. DESCRIBE THE RESOURCE AS COMPLETELY AS POSSIBLE:

a) Overall shape
stories square; two-and-one-half
bays three
wings north (rear) facade contains one story shed addition

b) Structural system frame

c) Foundation
materials brick
basement yes - bulkhead located on east facade

d) Exterior walls (modern over original)
materials vertical board siding
color(s) grey

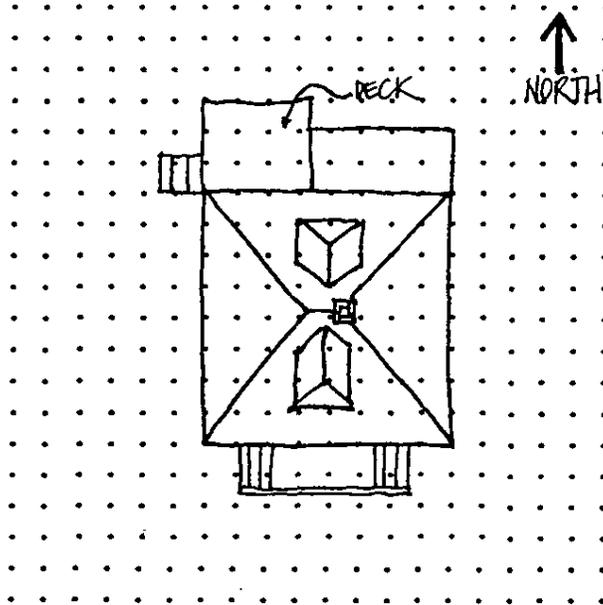
e) Roof
shape; materials hipped; asphalt shingles
cornice wide beaded board eaves and wide fascia board trim
dormers one each on north and south facades, hipped with tripled one-over-one
chimney location(s) double hung windows. Chimney - center roof ridge, brick.

7. DESCRIPTION (cont'd):

CRS # N-12913

- f) Windows
spacing regular
type single and paired, one-over-one double hung
trim simple wood
shutters none
- g) Door
spacing south facade, center bay; rear sliding patio doors leading to deck
type single leaf, twelve light
trim simple wood surround
- h) Porches
location(s) south facade, central entrance bay
materials pressure treated wood
supports pressure treated wood posts
trim pressure treated wood balustrade, railing, and steps
- i) Interiors details (if accessible)
not accessible

8. SKETCH PLAN OF BUILDING:



INDICATE NORTH ON SKETCH

9. Surveyor: Jill Cremer and Martin B. Abbot Date of Form May 1993

USE BLACK INK ONLY



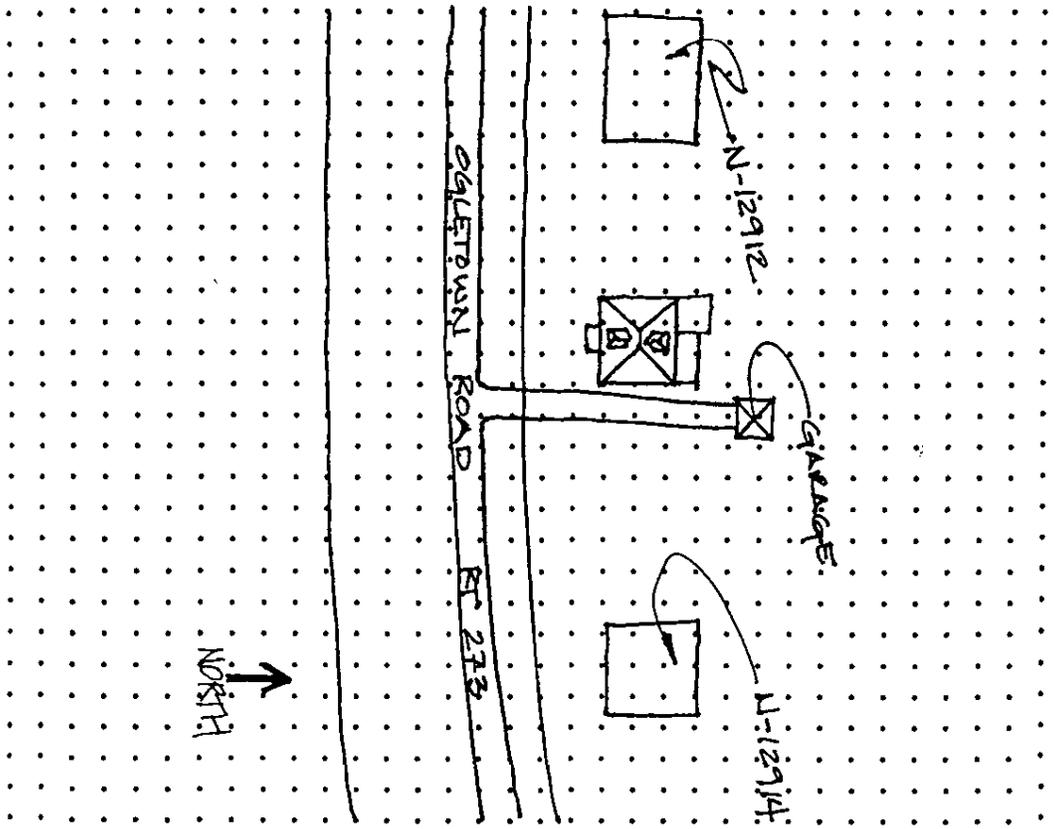
CULTURAL RESOURCE SURVEY
PROPERTY IDENTIFICATION FORM
FORM CRS-3

CRS no.	<u>N-12913</u>
SPO Map	<u>06-07-34</u>
Hundred	<u>Wh. Clay Cr.</u>
Quad	<u>Newark East</u>
Zone	<u>I</u>
Acreage	<u>.48</u>

1. NAME OF PROPERTY: 1708 Ogletown Road
2. STREET LOCATION: 1708 Ogletown Road, Newark, DE 19711
3. OWNER'S NAME: Claude R. and Eileen W. Moffett TEL. #: not available
ADDRESS: 1708 Ogletown Road, Newark, DE 19711
4. TYPE OF RESOURCE: a) building b) structure _____ c) site _____
d) object _____ e) district _____ f) other _____
5. SURROUNDINGS: (check more than one if necessary)
a) fallow field _____ b) cultivated field _____ c) woodland _____
d) scattered buildings e) densely built up _____ f) other _____
6. FUNCTION: original Single Family Residential present Single Family Residential/
Commercial
7. LIST ADDITIONAL SHEETS USED
CRS-7 Related Outbuildings Form (1)

8. SURVEYOR: Jill Cremer and Martin B. Abbot TEL. # 215-561-1050
ORGANIZATION: Kise Franks & Straw, Inc. DATE: May 1993
ADDRESS: 219 N. Broad Street, Philadelphia, PA 19107

Please indicate position of resource in relation to geographical landmarks such as streams and roads.



INDICATE NORTH ON SKETCH

USE BLACK INK ONLY

10. CONTEXT Consider the following:

- a) relationship to setting
- b) associated traditions or stories
- c) noteworthy features
- d) comparison with others in area
- e) threats
- f) additional documentation

This dwelling was constructed circa 1926 after an earlier dwelling located on the property was destroyed by fire circa 1925. Although the date of construction for the earlier building is not known, the current owner claims that a 1756 corner-stone was discovered beneath the former porch. The presence of an earlier home is supported by the 1849 Rea & Price Atlas of New Castle County which delineates a building at this location as belonging to G. Janvier. The 1868 Pomeroy and Beers, 1881 Hopkins, and 1893 Baist atlases identify J. Morrow as the owner of this property. New Castle County property records indicate that this property was sold to John F. Richards, who purchased approximately 300 acres from the Morrow family in 1911. According to Robert Stafford, Richards' grandson, the original farmhouse was part of a five acre parcel that Richards separated from his 300 acre farm, and subsequently rented to farmers. Originally, the property contained the circa 1756 house, one brick barn, one stone barn, and a chicken coop. The only extant resource is the chicken coop, which is located at 1610-1612 Ogletown Road (N-12912). Mr. Stafford recalled that the barns were torn down circa 1936, and the bricks from one barn were salvaged to build the inner walls of the dwelling at 1610-1612 Ogletown Road (N-12912). After Richards' death in 1947, the property was sold to his daughter and her husband, Mildred and Courland Nesbitt. (continued on page 2)

11. Comprehensive Planning:

- a. Time Period(s) 1880-1940 +/-
- b. Historic Theme(s) Settlement Patterns and Demo. Change

12. Evaluation and Eligibility

- a) area(s) of significance Appears Not Eligible
- b) criteria

13. Certification

PI: Name/Title Jill Cremer and Martin B. Abbot Date May 1993
 SHPO: Name/Title _____ Date _____



CULTURAL RESOURCE SURVEY
RELATED OUTBUILDINGS FORM

FORM CRS-7

CRS no.	<u>N-12913</u>
SPO Map	<u>06-07-34</u>
Hundred	<u>Wh. Clay Cr.</u>
Quad	<u>Newark East</u>
Zone	<u>I</u>
Acreage	<u>.48</u>

1. ADDRESS OF PROPERTY: 1708 Ogletown Road, Newark, DE 19711
2. FUNCTION: Garage
3. DATE: circa 1926
4. STYLISTIC FEATURES: two bay, open garage in Four square style similar to house
5. ARCHITECT/BUILDER: unknown
6. DESCRIPTION:
 - a) Structural System one story, frame
 - b) Wall Coverings asphalt shingles
 - c) Wall Openings
 - windows located within sliding cross braced garage doors
 - doors two vertical board and cross braced, sliding garage doors with four light fixed sash on south
 - other facade; no other.
 - d) Foundation not visible
 - e) Roof
 - structural system frame, pyramidal with exposed rafters in the eaves
 - coverings asphalt shingle
 - openings none
 - f) Interiors
 - floor Plan open

 - partition/walls none

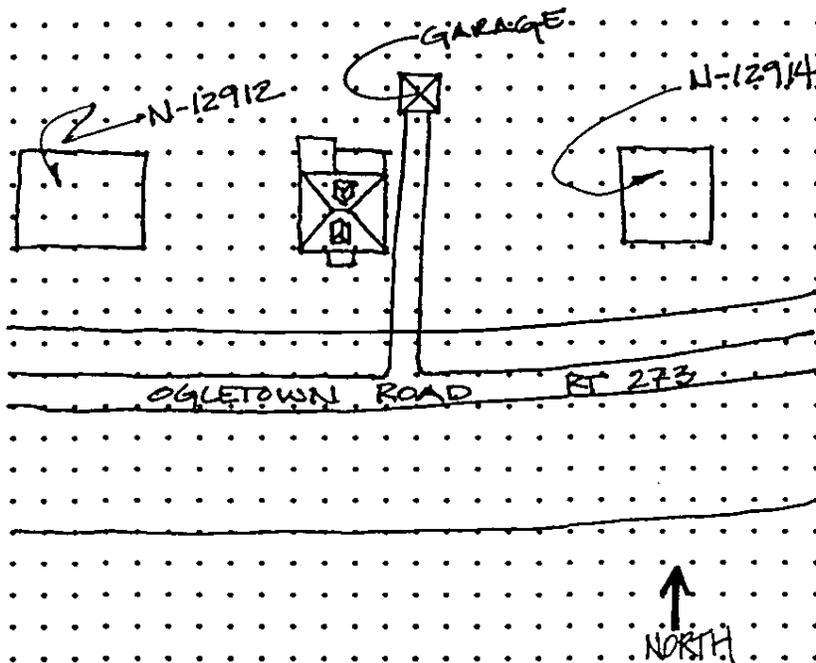
 - interior finish exposed frame and siding

 - furnishings/machinery none

7. SKETCH MAP

CRS # N-12913

Show relationship to main building and provide sketch plan of outbuilding.



INDICATE NORTH ON SKETCH

8. SURVEYOR: Jill Cremer and Martin B. Abbot

DATE OF FORM: May 1993

PHOTOGRAPHIC INVENTORY

BUREAU OF ARCHAEOLOGY AND HISTORIC PRESERVATION

Document 20-06/79/01/11

CRS Number N-12913 Date MARCH 1993 Contact Sheet # _____

Description 17 - VIEW EAST ALONG RT. 273 - RESOURCE ON LEFT. 19 - DWELLING, VIEW NORTHEAST. 20 - DWELLING, VIEW NORTHWEST. 21 - GARAGE, VIEW NORTH.

Location (if other than Bureau collection) _____

Attach contact print



PHOTOGRAPHIC INVENTORY

BUREAU OF ARCHAEOLOGY AND HISTORIC PRESERVATION

Document 20-06/79/01/11

CRS Number N-12913 Date MARCH 1993 Contact Sheet # _____

Description 22 - MODERN OUTBUILDING, VIEW NORTHWEST. 23 - DWELLING, VIEW SOUTH. 24 - DWELLING, VIEW NORTH-NORTHEAST. 25 - DWELLING, VIEW NORTH-NORTHWEST

Location (if other than Bureau collection) _____

Attach contact print





CULTURAL RESOURCE SURVEY
 CONSTRUCTION DATA FORM

FORM CRS-1

CRS no.	<u>N-12914</u>
SPO Map	<u>06-07-34</u>
Hundred	<u>Wh. Clay Cr.</u>
Quad	<u>Newark East</u>
Zone	<u>I</u>
Acreage	<u>.53</u>

1. ADDRESS OF PROPERTY: 1712 Ogletown Road, Newark, DE 19711

2. DATE OF INITIAL CONSTRUCTION: circa 1942

3. STYLE/FLOOR PLAN: Cape Cod Colonial

4. ARCHITECT/BUILDER: unknown

5. INTEGRITY: a) original site _____ b) moved _____
 c) if moved, when and from where _____
 d) list major alterations and dates (if known)
original one story shed addition on east facade covered with aluminum siding; modern
aluminum replacement windows throughout house; circa 1985 pole barn on north facade.

6. CONDITION: good _____ deteriorated _____

remarks: _____

7. DESCRIBE THE RESOURCE AS COMPLETELY AS POSSIBLE:

- a) Overall shape
 - stories square; one-and-one-half
 - bays three
 - wings one story shed addition on east facade; circa 1985 pole barn on north facade
- b) Structural system masonry
- c) Foundation
 - materials brick
 - basement not visible
- d) Exterior walls (modern over original)
 - materials common bond brick
 - color(s) red
- e) Roof
 - shape; materials side gabled; asphalt shingles
 - cornice box cornice and plain fascia board
 - dormers south facade - two front gabled dormers with six-over-six double hung
 - chimney location(s) windows and painted asbestos shingled cheeks; north facade - one full shed dormer covered with aluminum siding. Chimney - east facade center, exterior brick with simple brick cap.

7. DESCRIPTION (cont'd):

CRS # N-12914

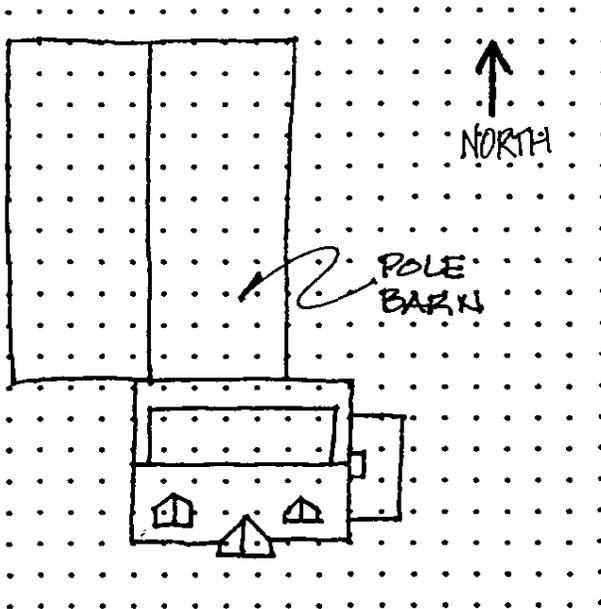
- f) Windows
 - spacing regular
 - type one-over-one, six-over-six, eight-over-eight double hung aluminum replacements with snap-in muntins. Trim - simple wood surround and brick sills
 - trim
 - shutters south facade first story, fixed aluminum shutters

- g) Door
 - spacing center south facade
 - type single leaf, two lights over four panels
 - trim fluted wood pilasters with molded caps support full entablature of molded cornice and triglyph and metope frieze.

- h) Porches
 - location(s) center bay south facade, front gabled door hood with semi-circular soffit
 - materials wood, asphalt shingle roof
 - supports molded brackets
 - trim brick steps and stoop

- i) Interiors details (if accessible)
 - not accessible

8. SKETCH PLAN OF BUILDING:



INDICATE NORTH ON SKETCH

9. Surveyor: Jill Cremer and Martin B. Abbot

Date of Form May 1993



CULTURAL RESOURCE SURVEY
PROPERTY IDENTIFICATION FORM

FORM CRS-3

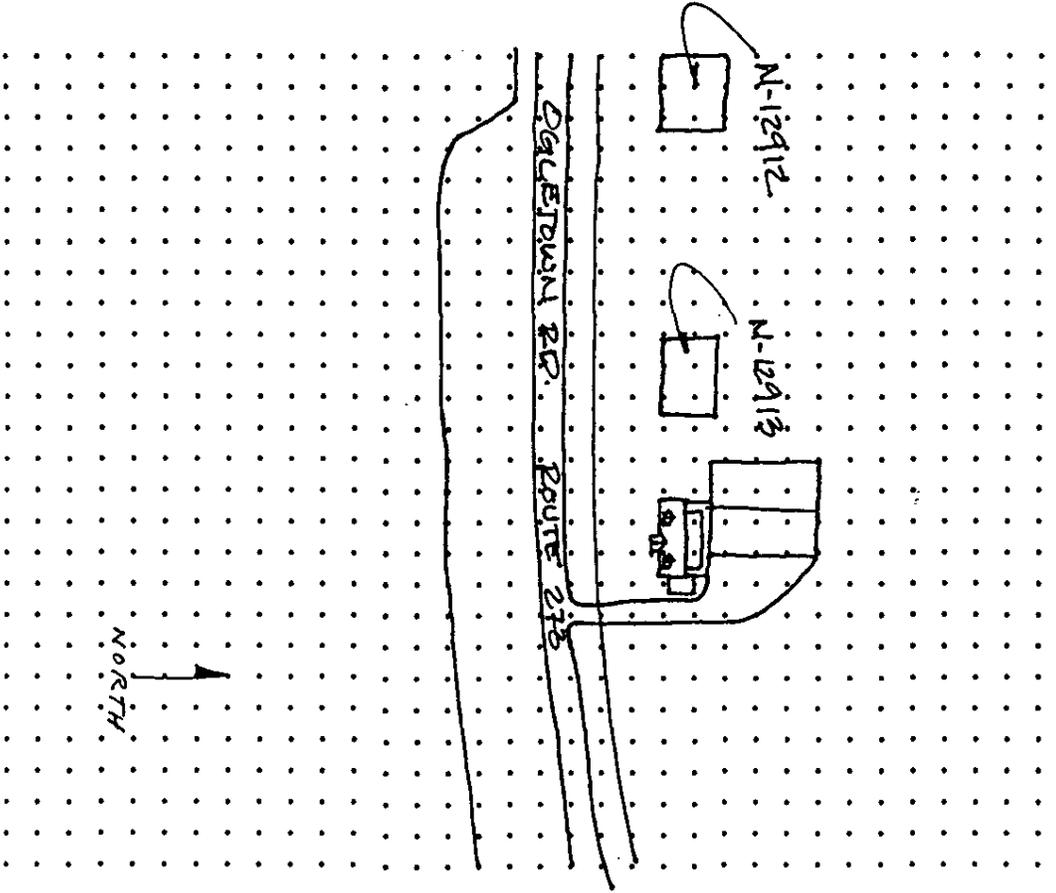
CRS no.	<u>N-12914</u>
SPO Map	<u>06-07-34</u>
Hundred	<u>Wh. Clay Cr.</u>
Quad	<u>Newark East</u>
Zone	<u>I</u>
Acreage	<u>.53</u>

1. NAME OF PROPERTY: 1712 Ogletown Road
2. STREET LOCATION: 1712 Ogletown Road, Newark, DE 19711
3. OWNER'S NAME: Paul L. and Dorothy J. Schlosser TEL. #: not available
ADDRESS: 540 Polly Drummond Hill Road, Newark, DE 19711
4. TYPE OF RESOURCE: a) building x b) structure _____ c) site _____
d) object _____ e) district _____ f) other _____
5. SURROUNDINGS: (check more than one if necessary)
a) fallow field _____ b) cultivated field _____ c) woodland _____
d) scattered buildings s e) densely built up _____ f) other _____
6. FUNCTION: original Single Family Residential present Commercial
7. LIST ADDITIONAL SHEETS USED
None
8. SURVEYOR: Jill Cremer and Martin B. Abbot TEL. # 215-561-1050
ORGANIZATION: Kise Franks & Straw, Inc. DATE: May 1993
ADDRESS: 219 N. Broad Street, Philadelphia, PA 19107

9. SKETCH MAP

CRS # N-12914

Please indicate position of resource in relation to geographical landmarks such as streams and roads.



INDICATE NORTH ON SKETCH

USE BLACK INK ONLY

10. CONTEXT Consider the following:

- a) relationship to setting
 - b) associated traditions or stories
 - c) noteworthy features
 - d) comparison with others in area
 - e) threats
 - f) additional documentation
- John F. Richards purchased this property from the Morrow family in 1911 as part of a 300 acre farm. In 1941 he sold this tract of land to his daughter and her husband, Mildred and Courland Nesbitt. According to Richards' grandson, Robert Stafford, this dwelling was built circa 1942 by the Nesbitts. The dwelling's mid-twentieth-century Cape Cod Colonial style is typical of suburban development at the time. Features common to the style include the overall symmetrical form and fenestration, the side gabled roof with front dormers, the side porch, and the central entranceway with a classical surround and decorative door hood. However, post-1950's alterations, such as modern replacement windows and the addition of a large pole barn on the rear of the dwelling have severely compromised the architectural integrity of the resource.

11. Comprehensive Planning:

- a. Time Period(s) 1880-1940 +/-
- b. Historic Themes(s) Settlement Patterns and Demo. Change

12. Evaluation and Eligibility

- a) area(s) of significance Appears Not Eligible
- b) criteria

13. Certification

PI: Name/Title Jill Cremer and Martin B. Abbot Date May 1993
 SHPO: Name/Title _____ Date _____

PHOTOGRAPHIC INVENTORY

BUREAU OF ARCHAEOLOGY AND HISTORIC PRESERVATION

Document 20-06/79/01/11

CRS Number N-12914 Date MARCH 1993 Contact Sheet # _____

Description 26-DWELLING, VIEW NORTH-NORTHEAST. 27-DWELLING, VIEW NORTH.
28-SAME. 29-DWELLING, VIEW NORTHWEST. 30-DWELLING AND POLE BARN, VIEW SOUTH.

Location (if other than Bureau collection) _____

Attach contact print



PHOTOGRAPHIC INVENTORY

BUREAU OF ARCHAEOLOGY AND HISTORIC PRESERVATION

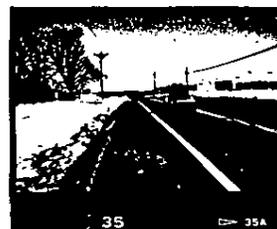
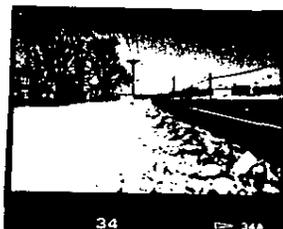
Document 20-06/79/01/11

CRS Number N-12914 Date MARCH 1993 Contact Sheet # _____

Description 32-VIEW WEST ALONG RT. 273- RESOURCE ON RIGHT. 34-VIEW
EAST ALONG RT. 273- RESOURCE ON LEFT. 35-SAME.

Location (if other than Bureau collection) _____

Attach contact print



APPENDIX B

VITAE

Martin B. Abbot graduated from Cornell University in 1988 with a B.S. in Urban and Regional Studies. Mr. Abbot has been employed as a Historic Preservation Specialist with Kise Franks & Straw (KFS) in their Historic Preservation Group since January 1990. Previously, Mr. Abbot served for two years as Historic Preservation Planner with the Delaware County Planning Department in Media, Pennsylvania. While at KFS, Mr. Abbot has developed Section 106 reports for the New Jersey Department of Transportation and the Baltimore District of the Army Corps of Engineers.